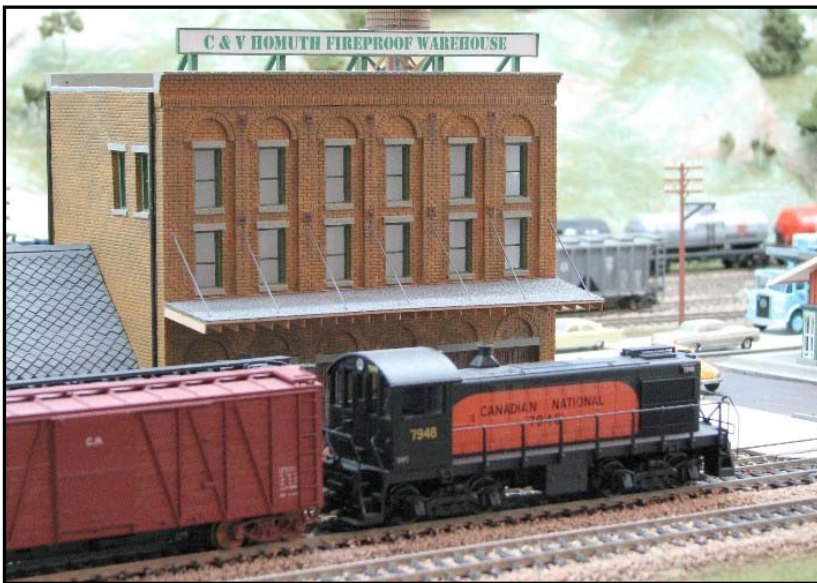


Two New Series: Building FreeMo Modules Building a Portable Display Layout



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Cover Photo: Top Left

CN Alco switcher moves cars past warehouse named for Gary Shurgold's longtime friend Carl Homuth. Photo by John Johnston

Cover Photo: Bottom Left

The S Scale Workshop was the largest modular layout at Copetown. The engines were equipped with the new Soundtraxx Tsunami. This module was built by Andy Malette. Photo by Peter Moffett

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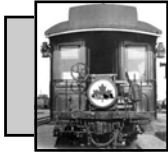
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THE OBSERVATION PLATFORM

NEW LAYOUT BUILDING SERIES

In this issue we are starting two new series on building portable layouts. Though they are both HO, the techniques can be applied to any scale. Why two? It is because they offer two different approaches to modular layout building. The series by Randy O'Hara will focus on FreeMo which is becoming extremely popular with modular groups. The series by Ted Rafuse will focus on a self contained modular display layout focused on showcasing a specific prototype and location.

I will be among the interested readers as my N Scale work group is actively looking at building a group of modules which will incorporate both approaches. It will be a self contained layout based on a specific prototype but will have FreeMo connections at its ends.

MODEL RAILROADING AND FRIENDSHIP

My wife and I headed south at the beginning of February for a month in South Carolina. As a result of my layout being on Jerry Britton's PRR Website I had been corresponding with a number of model railroaders in central Pennsylvania who shared similar interests. and an invitation to spend a couple of days railfanning and layout visiting was extended to us. Bill Lewis of the PRR Historical Society was our host and we spend a delightful evening with him and his wife, enjoying dinner and talking railroading and modeling. The next day, Bill acting as both host and driver took us to Todd Treaster's beautiful N scale layout which is featured in this months N Scale Railroading. Todd is a Helper engineer for Norfolk Southern on the East Slope of the Horseshoe Curve and it was an excellent opportunity for me

to pick his brains and enhance my knowledge of how my PRR Horseshoe Curve layout will operate. We then visited Jerry Britton's layout. Some of you may remember Jerry's layout from Model Railroad Planning 2006. The layout is centered on the Harrisburg Station. We were welcomed with open arms and had an enjoyable day. Over the last 40 years I have found this to a common occurrence with model railroaders, their willingness to open their homes, share their time, their skills, and their accomplishments with their fellow modellers. I'm sure it happens in other hobbies, but I'm not sure that it happens to the extent that it does in ours. Thanks guys!

CORRECTIONS TO THE LAST ISSUE

It appears that there were a number of errors in the article on Bob Duncan's Prince Edward County Railway, starting with the title. I called it the Prince Edward Island Railway which is clearly wrong. Second, the photo included with the article was that of fellow Cobourg modeller, John Johnston (yes, my namesake). Our apologies to Bob for the mixup and below is a photo of the real Bob Duncan. Ted Rafuse tells me that the group had some fun with the mixup and at their regular meeting Bob's wife Sally came down to the basement to receive John with a great big hug and a wonderful kiss to welcome him home. She wondered if he needed some TLC. The group got a



good chuckle and razzed them both thoroughly for the remainder of the night.

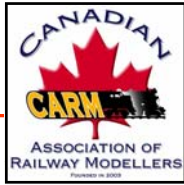
PRINTING QUALITY OF THE LAST ISSUE

The print quality of the last issue did not meet the standard that I have set for the publication of The Canadian. This problem rested with the print shop. I will spare you the technical problems that were encountered, however it is sufficient to note that we have changed print shops and that the tests we ran with the new contractor showed that they will provide us with a high quality product.

I did receive some e-mails which pointed out the problem and detailed responses were sent to those individuals. One individual chose not to address the problem directly with me but rather to post a rather vitriolic message on a Yahoo list. This type of response is harmful to CARM as an organization. We welcome feedback, I welcome feedback, however the flip side of that is our expectation that if you have a concern or problem that you address it to us and give us a chance to respond and that it be done in a courteous and civil manner. The Canadian is a newsletter not a newsstand publication and we are unpaid volunteers, some like myself, learning as we go along. There will be bumps in the road where we don't meet your expectations or ours but we will learn from those and move forward.

In the same post there were scathing remarks about the quality of some of the photographs submitted by some of our authors. I want to let our contributors both present and future know that we appreciate the fact that they have taken the time to share their work with us. While high quality photographs are clearly preferable, if a photo is important and useable we will utilize it. Our goal is to showcase our members work, not to sell magazines. We will always err on the side of supporting our members.

John Johnston, Editor



National News Page

1ST CARM MANUFACTURER'S SURVEY

Overall we received 80 responses to our Survey. This is an excellent response and I thank all of you who participated. 90% of the responses were from HO Scale modellers with the rest spread over the other Scales.

I have analyzed the returns and have several general observations:

1: Many of you took the time to add comments and notes about prototype fidelity. The hobby is clearly at a stage where people are looking for more than a generic locomotive with Canadian National on the tender or painted in Canadian Pacific grey and maroon.

2: The range of suggestions was almost as broad as the number of people sending in ballots. As a result, in some categories I will make a broad observation as well as list the top vote getter(s).

3: A significant number of people mentioned DCC and Sound. Both are clearly establishing themselves in the hobby.

HO SCALE STEAM ENGINES

94 votes cast
69 votes were for Pacifics/Mikados or smaller. 25 votes were for Berkshires or larger.

- #1: CPR 4-6-0
#2: CNR 4-6-0
#3: CNR 2-6-0 #3: CPR 2-8-2

HO SCALE DIESELS

83 votes cast
67 votes were for 1st and 2nd generation diesels. 16 votes were for post

1980 diesels.

- #1: CN/CP chopnose GP9's
#2: CN GMD1
#3: CP RS23
#3: FPA2/4, FPB2/4 in VIA &CN

HO SCALE FREIGHT CARS

66 votes cast
46 different cars were identified in the 66 votes.

- #1: CN&CP 8 Hatch Express Reefer
#2: CN 56ft Double Deck Auto Car
#3: Gondolas of many types:
 CP Otis, CP GS, CN Hart,
 CN Mill, CN GS

HO SCALE PASSENGER CARS

60 votes cast
42 different cars were identified in the 60 votes.

- #1: CPR/VIA "Park" Series

The only other common thread was baggage cars in CPR/CNR/VIA/NAR or ONR paint schemes.

HO SCALE NON REVENUE

37 votes cast

Two types of cars in a wide number of variants dominated 75% of this category.

- #1: CN Caboose
#2: CP Caboose
#3: CN or CP Snow Plows

HO SCALE STRUCTURES

54 votes cast
54 different structures identified. There was some commonality on one type of structure and a desire for buildings that are uniquely Canadian such as Canadian Tire or Tim Hortons.

- #1: Prototype Canadian stations.

Among the railroads identified were CN, CP, BCR, TH&B, NAR

HO SCALE OTHER ITEMS

30 votes cast

- #1: Vehicles of every era and type

N SCALE

There were only 56 votes cast across the 8 categories in N Scale so it was not possible to draw any conclusion from the results.

The only item to receive more than 1 vote were CN and CP 4-6-0's with each receiving 2 votes. It is worth noting that almost half of the 15 votes in the steam category were for large steam such as a Royal Hudson, Selkirk, Northern, Berkshire or Mountain.

O SCALE - With only 22 votes cast in the 8 categories, no conclusions could be reached.

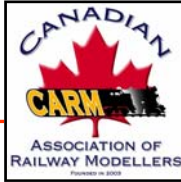
S SCALE - With only 12 votes cast in the 8 categories, no conclusion could be reached.

NARROW GAUGE - With only 13 votes cast in the 8 categories, no conclusion could be reached.

G SCALE - With only 3 votes cast in the 8 categories, no conclusions could be reached.

NOTE: Only one model was mentioned in each of HO, N, O, and S Scales. The CPR D10 4-6-0.

We will be looking at how best to communicate this information to manufacturers and sharing our findings with CNSig, CPSig, and Canadian Railway Modeller.



Chapter News Page

ANNOUNCEMENT

A new Chapter to be known as the **National Capital Chapter** has filed the paperwork for a Charter. Welcome aboard everyone! **The next Chapter meeting will be June 14.**

UPCOMING CHAPTER MEETINGS

In future issues, I will list the dates of Chapter meets I know about at the top of this area. Detailed information will be found in the Chapter Report.

April 25: London & Area
April 29: Ontario MidWestern
May 3: Toronto
May 12: Golden Horseshoe
June 14: National Capital
June 15: Toronto

CARM COPETOWN SHOW

The 11th annual Copetown Train Show was held on February 25. The show featured modular layouts, Canadian manufacturers, photo vendors, SIGS, model railway clubs and historical Societies. The Ontario Central and Golden Horseshoe Chapters presented workshops on building locomotives and weathering. The weather was good and resulted in a good turnout. Many CARM members stepped forward to volunteer and make the show a success. They were Reg Laprise, Bill Bradford, Gord MacBride, Randy O'Hara, Steve Hoshel, Ron Pullano, Jim McInnes, Bob Melvin, Bill Osborne and Shaun Burns.

The efforts of all of these individuals resulted in a net profit for the show of \$1138 as well as \$600 split between the Golden Horseshoe, Credit Valley, and Ontario Central Chapters.



Above Left: Bill Bradford of the Ontario Central Chapter presented a workshop on building and detailing locomotives. Bill's presentation lasted all day and was very popular. Peter Moffett photo.



Above Right: Ron Pullano of the Golden Horseshoe Chapter also presented an all day workshop. His subject was weathering. Peter Moffett photo.

Below: Another scene on the S Scale Workshop modular layout. This layout was built on the FreeMo modular concept. This module was built by Simon Parent. Peter Moffett photo.



GOLDEN HORSESHOE CHAPTER

The Golden Horseshoe Chapter held its annual meeting at the HOMES model railroad club in Stoney Creek on Saturday January 6, 2007. Members of the National Executive were present. Pete Moffett – Acting Chair of the Golden Horseshoe Chapter

introduced each member of the National Board.

The purpose of this meeting was to elect a new executive for the Golden Horseshoe Chapter. Two positions were up for election – Chair and Secretary/Treasurer. There was no one willing to put their name forward for Chair. Tom Allan allowed his name

to stand for Secretary/Treasurer.

John Johnston, National Board Chair, addressed the members and told them that the position of Chair needed to be filled. The commitment of time is not great. Each Chapter is expected to have two meetings a year. This meeting counts as one and there is a second meeting planned for May 2007, so the work for this year is basically done. He also advised that this is the largest chapter in the country but unfortunately one of the least active. He asked that those present consider taking on this task.

Tom Allan was thanked for allowing his name to stand for Secretary/Treasurer and since he was the only name nominated Tom was acclaimed to this position. The CARM Exec will assume the position of Acting Chair until a Chairman is found.

After the meeting, members enjoyed a day of clinics on subjects like DCC, scenery, airbrushing with acrylics and town planning. The afternoon featured home layout tours and the HOMES club layout.

Golden Horseshoe Chapter Meeting Announcement

Ontario, St. Catharines, May 12. Golden Horseshoe Chapter joint meeting with International Division (NMRA), Niagara Division (CRHA) featuring clinics in the morning, layout visits in the afternoon, informal dinner in the evening. Doors open 8:30am meeting starts 9:00am sharp. Lock 3 Museum, Welland Canals Parkway and Queenston Street, St. Catharines. Admission \$2.00. For information contact Tom Allan ph: 905-575-9326 email: tomallan@mountaincable.net

ONTARIO MIDWESTERN CHAPTER

On April 29th 2007 The Ontario Midwestern Chapter is having their spring meet at the Palmerston Railway Museum. Because of this unique opportunity we have decided

to invite all members of CARM to participate. The admission fee is by donation and all proceeds will go towards the museum. Time will be 2 pm for our regular meeting and 3:30 pm for the rest of the CARM family. If you want to check out the web site the link is <http://www.angelfire.com/co4/palm/> See you there

EDITOR'S NOTE: Randy O'Hara the Chairman of the Ontario Midwestern Chapter sent me an e-mail outlining the approach his Chapter is taking. With his permission I am sharing it with all of you. It gives a good insight into what a CARM Chapter can accomplish.

A good reason to join CARM is for fellowship...Peter joined CARM and met Art and Brian...Although Peter already had a crew operating his layout Peter never knew Art and Brian existed close by. Once they met through CARM they became instant friends and now hold an operating session once a month together. If there ever was a reason to join CARM that would be it...But wait there's more...We started a new program called appreciations...What it is, is we set up a layout visit at a person home who is not a CARM member (hopefully) and give them a laminated appreciation letter for having us over, and an application form to join CARM before we leave. (We also take their photo receiving the letter). We all know or heard of someone who has a layout that probably is not a member. Hopefully with our last visit the fellow phoned me the next day and said he has all intentions of joining and he has two to three other friends who also may join. That's terrific news.

Now of course you are also aware we are starting free-mo. I have one module assembled and I am getting together with a couple of our members next month to build another. This will be photographed for the next edition of the Canadian. Also from the funds of last years train show we are building two corner modules which will be owned by the Chapter. Here's the

thing not everyone wants to build modules. So I came up with the idea that we will build these modules in steps, each member doing a part of the build, this way a lot of people get involved and not really feeling over stressed.

Then it is our intentions to visit other Chapters and encourage them to build Chapter modules. (more on that later) We are having a spring meeting at Palmerston Railway Museum (Which is open to CARM members) and have invited the Ontario Central Chapter to join us for a joint meeting. A similar meeting is being planned for the London and Area Chapter. I feel it very important that we get to know our neighbors. Sometime this early summer I am working on trying to get access to a couple of big layouts in the Guelph area.

Some of us are setting up our own Train show....The Day at the Clinics will be moving in 2008 to its own venue. Closer to Central Ontario and a bigger place. It is my intentions to have a show very similar to your Copetown. We may buddy up with another event to be named later. This long list allows people to volunteer or work on stuff their interested in. I'm not sure what else I can add to this long letter but you can see that we are a very active Chapter.

Randy, Chairman

Ontario Midwestern Chapter of CARM

TORONTO CHAPTER

The rehabilitation of the Sick Children's layout has been completed. A meeting was held on February 15th with Pawlo Alaanskyj speaking on V Scale. (video modeling) On March 31 the Chapter will be running a guided tour to the Doubleheaders Layout Tour. We have a table at the Lakeshore Model Railroaders Flea Market which will be held at Humber College on April 1st.

We are running a tour of Union Station on Thursday May 3 at 7:30 in the evening. There is a charge of

\$10 per person by the Toronto Railway Historical Association to support the John Street Roundhouse project.

There will be a BBQ held at 6:00 pm on Friday June 15th, at 200 Manitoba Street in Etobicoke. Chapter elections will be held at this meeting.

ESSEX KENT CHAPTER

Here is some information from our recent Essex Kent Chapter "Mid Winter Layout Tour" which occurred on Saturday, February 10th. There were 5 HO layouts visited in the Essex / Comber / Kingsville & Leamington areas. The outing was well attended by 18 train buffs from our Chapter area and also the London vicinity. Please find included some pictures taken by one of the members during the day. Due to the success of this tour, interest has been expressed by our group in having a "Spring Layout Tour" in the Chatham / Kent area.

LONDON AND AREA CHAPTER

A meeting was held on January 10 and the **next London and Area chapter meeting will be April 25, 7:30 at Dave Minshall's home.**



Photo Above: Members of the London and Area Chapter meet on January 10.

Photo Right Centre: Bill Johnson, Don Eastman, John Eydt, Ken Nicholson, and Dave Ladore on Essex Kent Chapter Layout Tour. Bob Sanford photo.



Photo Right Bottom: Little River Lumber Company on J. Whittle's layout on the Essex Kent Tour. Dave Ladore photo.

Photo Left Bottom: "A somber day downtown" on Bill Knox's layout on Essex Kent tour. Dave Ladore photo.

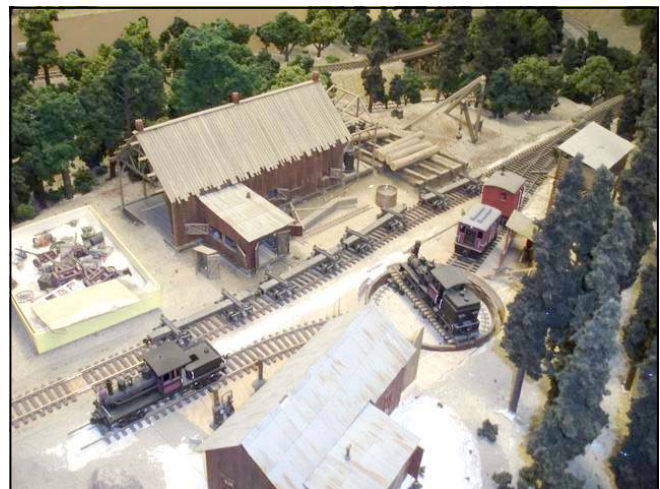




Photo Left Above: The coal yard on Bob Henderson's layout on Essex Kent tour. Bob Sanford photo.

Photo Left Below: Welcome to the Norman Bates motel on Mark Roach's D&H layout on the Essex Kent tour. Bob Sanford photo.

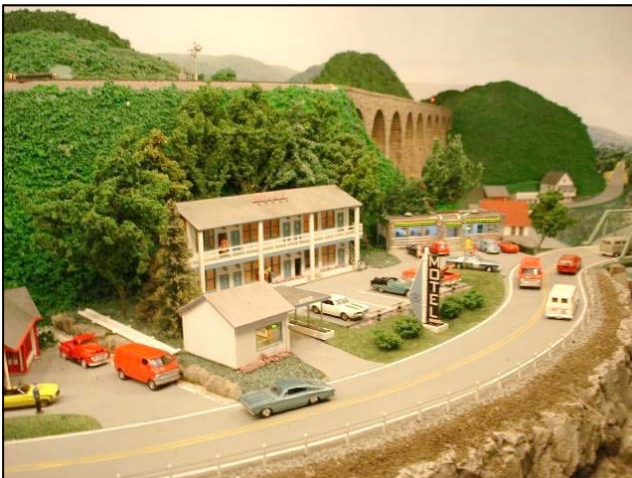


Photo Right Above: Work in progress on Mark Roach's D&H layout on the Essex Kent tour. Bob Sanford photo.

Photo Left Below: "One more load California bound" on J. Whittle's layout on the Essex Kent tour. Dave Ladore photo.





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Canadian Railway Craftsman Track Three: Proposal for Change by Dave Burroughs, CRC Manager

Introduction

A few months ago, I received a call from Randy O'Hara representing the Mid Central Chapter. Randy asked if there were any provisions in the Layout Builder Track for those who were limited in space. CARM is a young organization and is willing to listen to the members. I put together a proposal to the Board of Directors for changes to accommodate those who are space limited. The proposal reduced the square footage requirements of the model railroad but came at a price. For those opting for the space limited submission, additional requirements were included in the proposal to maintain equality with the member who submits the full square footage of layout to be judged.

The CARM Board of Directors reviewed my proposal at several BOD meetings. After discussion, the decision was made that it would be best if input from the membership were solicited.

The proposal allows for space limited members to build modular sections of a model railroad with reduced requirement for track and size of the modules they are submitting to have judged for Track Three. The problem comes in setting the requirements for square footage and quantity of track contained on the modules for different scales.

We are asking for input from as many of you as possible, especially those who might be most affected by this proposal, as to the specifications on size and linear footage of track that would be a fair substitute for a home layout. The real issue is the quantity of work that would be required from the space limited member. How much track should there be and how much scenery? Should the amount of work be proportional for the scale or should it be a fixed

quantity for all scales?

The next two sections of this article are the actual proposal. The Space Limitations for Track Planning would be added below page 1 of the Track 3, Track Planning requirements as found on the CARM web site. The Space Limitations for Scenery would be added below page 1 of the Track 3, Scenery requirements as found on the CARM web site.

Please review this **proposal** and send your comments to me at as soon as possible. Please include your membership number with your feedback.

Track 3, Track Planning Proposal

SPACE LIMITATIONS

*For members who have space limitations that prevent them from building a permanent layout, modular layout sections may be considered for qualification for this Track with the following modifications to the requirements:

For item 1) above, the track plan submitted may be a design for a proposed model railroad which incorporates all of the necessary features.

For item 2) above, the combined linear footage of track required is reduced to 15 feet in N scale, 30 feet in HO scale, 45 feet in S scale, and 60 feet in O scale. The track should in-

corporate as many features in 1) above as space permits.

For item 5) above, the addition of item g). If submitting modular sections, include a track plan of all modular sections identifying the features as in 5)b to 5)f

To qualify for the Space Limited exemption, the required modular sections must be exhibited in one club or public event.

Track 3, Scenery Proposal)

SPACE LIMITATIONS

*For members who have space limitations that prevent them from building a permanent layout, modular layout sections may be considered for qualification for this Track with the following modifications to the requirements:

For item 1) above, the completed square footage of combined modular sections will be reduced to 12 sq. ft. in N scale, 24 sq. ft. in HO scale, 36 sq. ft. in S scale and 48 sq. ft. in O scale.

To qualify for the Space Limited exemption, the required modular sections must be exhibited in one club or public event.

These new changes will open up avenues for more members to partake in Track Three, Layout Builder. Most

members can find space to store one module while under construction. With the low profiles of the Free-mo modules and such, one could hide it behind a couch in the living room if needed. Once a module is completed, a call for judging should be coordinated, if possible, for the day of the exhibition of the module. The necessary paperwork should be completed and presented to the judge on the day of the exhibition.



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Canadian Railway Craftsman Track Five: Service

by Dave Burroughs, CRC Manager

Track Five – Service

This Track really speaks for itself. The purpose of the Service Track is to give you an opportunity to serve your fellow modellers, the hobby in general and the Association by sharing your time and expertise. While putting forth service to the CARM community, the applicant expands his/her horizons in the hobby at the National and local levels.

Requirements for the Service Track:

Members must participate in at least 4 conventions, one being a National Convention or optionally, must participate in one National Convention and a minimum of three non-convention volunteer categories.

National level service:

National Convention duties may include acquiring hotel and convention facilities, clinicians, door prizes, contest chairman, publicity coordinator, etc. Other items that qualify are setting up tours, layout visits, prototype excursions, displaying modules at the convention, opening your layout, clinician, manning CARM booths, preparing registration envelopes, signage and maps. Serving as a committee chairman, committee member, security chairman or registrar also qualify as participation examples on a National level. Of course, local conventions require all of the above but generally to a lesser degree of intensity.

Local level and other volunteer areas of service:

There are lots of things that you can do on a local level to qualify for other volunteer areas. Membership promotion, manning a CARM booth at a flea market or train show, presentations to local youth groups, Scouts, schools, community centres, hospital and senior citizen groups, churches and the list goes on. You may also

volunteer to assist in the CRC program by judging for Track qualifications. Other positions include Editor for The Canadian, distributor for The Canadian, Membership Coordinator, Secretary, Webmaster, Chapter President or Secretary/Treasurer.

Live clinics presented at a Chapter meet or Convention must be a minimum of 30 minutes in length. To qualify, you must prepare and present your own material, hand out a presentation of the clinic material to the attendees and attach a copy of the clinic material to your Track Qualification Form. A live clinic may be claimed only once even though it may have been presented multiple times.

Multi-media clinics are video or multi-media presentations involving graphic material put together as a story or detailed presentation of a process. A multi-media travelogue with focus on modelling opportunities would qualify for submission for this track. A video presentation of a live clinic does not qualify as a multi-media clinic.

In the case of a dual person presentation for either type of clinic, each participant may claim half the credit provided that they did at least 40% of the work involved.

All multi-media clinics must also have a written handout for the attendees. A copy of the handout must be attached to their Track Qualification Form.

Verification:

Verification of all contributions to Track Five shall be acknowledged by a Convention official, or in the case of the public sector, the person who you arranged the presentation with.

Conclusion:

This Track is the culmination of one of the basic beliefs of CARM, service to your organization. Through this

Track, you will gain a feeling of accomplishment that you have given something to your peers and fellow members of this organization. Service is what will drive this organization forward and reward those who contribute.

Member Poll:

At a recent Golden Horseshoe Chapter meeting, I asked for a show of hands from the membership for those who were working toward their CRC. I would like to expand this Poll to a National level and ask for any member who has started working toward any of the Tracks to drop me an email with a quick summary of your progress.

Once again, I encourage you to document your accomplishments on the Qualification Forms. You will be surprised how much you can already claim without doing any additional work. If you feel that there are a couple of Tracks that you may find difficult, you should still submit for the other Tracks. Keep in mind that CARM has a mentor program to assist you with areas of the hobby that you may find more challenging. If you identify a need, then we will do our best to assist, even if it means publishing requests for mentors in a specific location.

As always, questions may be directed via email to crc@caorm.org or via Canada Post to:

CRC Manager
Canadian Association of Railway Modelers
373 Cranbrook Drive
Hamilton, ON, L9C 5W1

Next issue, we will investigate the last Track in the CRC program, and one that you have all started to work on, believe it or not, Track Six - Craftsman Model Builder.

Introduction to FreeMo Modules by Randy O'Hara

The Ontario Midwestern Chapter at the Oct 21 2006 annual meeting decided to incorporate modules into their activities. After a good discussion it was decided that a module system called Free-mo would be the way we would go. If you are familiar with Free-mo you are also aware that it is possibly the fastest growing interest in the Model Railroad hobby right now.

Free-mo is still fresh and to date there are a very limited number of people in our Province of Ontario doing this type of modules. Free-mo allows people from different areas globally to get together and have operating sessions, and there are currently no sessions right now in Ontario. Also it is something we plan to incorporate into our yearly show "Day at the Clinics" I have put together an introduction of Free-mo for those who are not familiar with this system.

INTRODUCTION

First was FREMO. This is a European Standard. FREMOdules was developed to put emphases on the modular endplates allowing greater flexibility in the modular design. Free-mo is the North American version of FREMO.

Free-mo is the latest in modular model railroading. Free-mo offers the greatest flexibility of operation. Each time you set up, the standards allow for a different configuration. It is possible to run with a single track mainline in a point to loop, loop to loop, or point to point.

The end result layout can be what ever size fits your space. I have seen photos of halls being used, and up to filling an entire arena. The free-mo specs of track centered at twelve inches on a twenty four inch end plate allows the modules to be switched 180 degrees. The wiring specs also allow for this.

Free-mo allows the best flexibility by allowing the modeller to focus on scenery and track flexibility. The specs define the width of the module, height of the module and where the track is to be placed. This leaves the length and track configuration up to the modeller. FreeMo allows the modeler to plan and model like he would a regular layout.

I suggest measuring the available space you have in your vehicle that will transport a module and build it to fit. If you only have a trunk then maybe a two foot by two foot would fit. Then build it. Put forty five degree angles on it and you have a curve, but some people have larger vehicles and will be able to build to suit. This allows the most flexibility which can incorporate yards businesses and even towns.

Only Free-mo allows this flexibility.

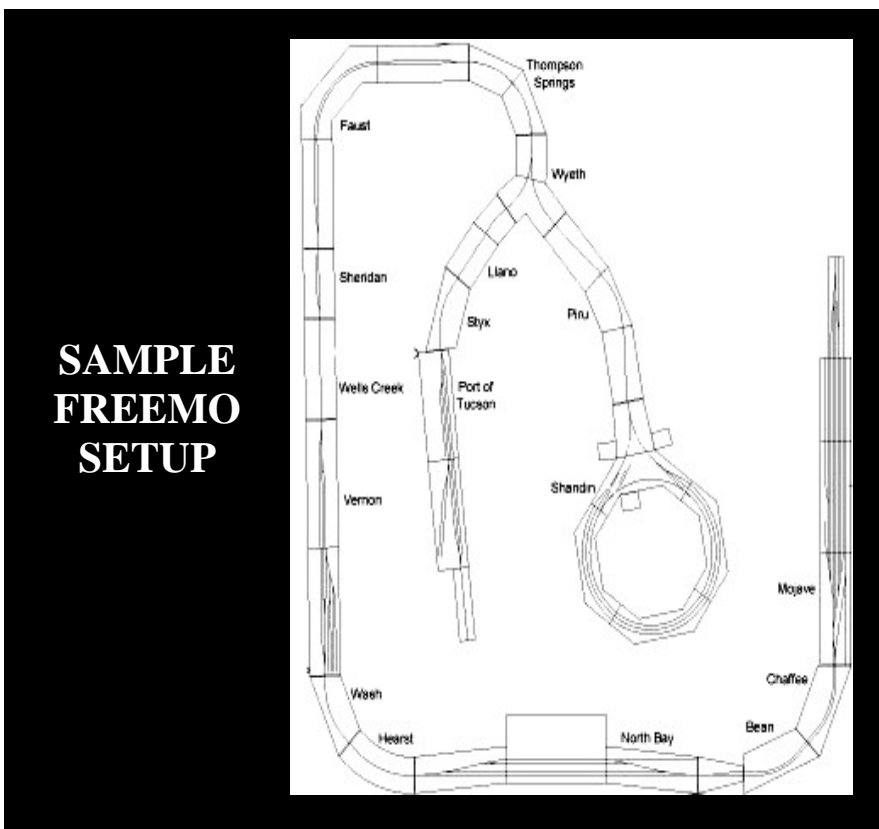
Although most modules are in HO scale I can't see why this can't be used in all scales. The interest I have encountered on the Free-mo concept is astounding. Whether you are in N scale or O scale I think this would work for you.

I encourage all Chapters to use this concept in their areas. We plan on not only showing them once a year but use the modules for promotion of our Chapter.

Just think. Now you can build a module to suit your own needs. More information can be obtained at these web sites or just google Free-mo.

<http://www.free-mo.org/index.html>
<http://www.calgaryfreemo.ca/>

Next edition we'll show you how to design and build the basic framework for a six foot module



Become a contributor to The Canadian

WHAT ARE WE LOOKING FOR?

The Canadian is the newsletter of the Canadian Association of Railway Modellers and our goal is to *share the work of our members*. To accomplish that goal we are looking for articles and photographs in the following categories.

- 1: Articles and/or photographs of your home layout, your club layout, or your modular sections.
- 2: Articles and/or photographs of National or Local events sponsored by or attended by CARM members.
- 3: Articles and/or photographs of your models. This may include construction information or may be a photo tour of models you have built.
- 4: Articles and/or photographs of railfan locations.

ARTICLES

The Canadian is prepared in MS Publisher. The best format to submit material to me is in MS Word. If you do not have MS Word, the alternative is to put the article in the body of an e-mail, or to handwrite it. Do not be concerned about spelling, grammar, or formatting. Those are my responsibility as Editor. I do not have the capability to read material which is sent in Word Perfect or word processing programs which are unique to a Macintosh.

PHOTOGRAPHS

Three types of photographs can be submitted. Print, slides, or digital. Prints or slides which are submitted will be returned if the sender requests it. Your full name and mailing address should be included with the photographs. I prefer you do not send scans of slides or prints unless you have access to a high quality scanner. Digital photos should not be retouched. Please send in the raw images from the camera. This helps

with the print quality when they are published. Size reduction increases the grain unless you intend them only for use on the Internet. Cropping and adjustments to brightness and contrast will be handled by myself depending on the space I need to fill. I have excellent photo editing software.

HOW TO SUBMIT MATERIAL

Addresses for both land mail and e-mail are at the bottom of page 2 of every issue. If you are sending digital images by e-mail, do not exceed 5Mb per e-mail. My ISP is Sympatico and my mailbox size is such that e-mails in excess of this size will be returned to the sender. If your e-mail is rejected I am notified and I will contact you by e-mail and make arrangements to be online to download your material so that mailbox size is not exceeded.

LAYOUT ARTICLES

These are the most popular type of article and I would recommend the following:

- Identify the scale being modeled.
- Identify the name of your railroad and whether it is free-lanced or follows a particular prototype.
- Provide statistical information on the layout such as size, type of trackwork, control system, number of operators etc.
- If your layout has unique features of which you are proud, identify those and describe them. This may include scratchbuilt structures, unique scenic features, handlaid track, etc.

With respect to photographs, I have two suggestions. Look at layout photos in the national magazines to get a sense of good composition. The majority of these photos are taken by professionals and we can learn from them. Second, if you are using digi-

tal, shoot the photographs without flash. Flash provides uneven lighting. It is easier to adjust a dark digital photograph with even lighting than one in which part of the photograph is dark and the rest is light.

HOW TO ARTICLES

When writing a How To article make the assumption that you are submitting it to one those popular Books for Dummies. Document every step, and do not assume that the reader will automatically know that is what you do. Try to photograph each step. If possible use a white background behind the model so that there are no other distractions in the photographs.

RAILFAN ARTICLES AND PHOTOGRAPHS

The Canadian is first and foremost about modelling. Many of us, however are also railfans. Publications like those put out by CNSig and CPSig do a good job of documenting rosters and we try not to duplicate their efforts. What I am interested in is safe railfan locations in your area that we can inform our members of if they are traveling. This should include directions on getting there and photographs taken at the location.

EVENTS

We are looking for photographs of CARM members at model railroad events. Identify the event and the names of the CARM members.

DO YOU HAVE A PHOTO, AN ARTICLE OR A TIP YOU WOULD LIKE TO SHARE?

SEND IT IN TO THE EDITOR EITHER BY MAIL AT

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41 GLENVIEW PLACE
HAMILTON, ON
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**OR BY E-MAIL AT
EDITOR@CAORM.ORG**



Designing the Cobourg & Peterborough Railway Part 1: Concept and Design

Article and Photos by Ted Rafuse

The idea of a train show display has had a long germination but a short construction period. For much of my life I have lived in Cobourg, Ontario, and as a teacher at a local high school I developed a local history curriculum. A facet of the course included an exploration of the development of water and rail transportation in the community. With a personal interest in railway history the Cobourg & Peterborough Railway held a degree of fascination. A variety of resources on the topic were sought and consulted. While I did not belabour the C&P for my students, my interest in this aspect of local railway history was maintained. This led to my writing of a C&P history manuscript, a project that currently remains incomplete and unpublished.

Having previously constructed several HO scale model railways, my current home layout is based upon the long defunct C&P. The original railway constructed between 1853 and 1855 had by the end of the nineteenth century lost most of the evidence of its existence. With modeller's licence my basement layout represents the C&P as though it may have survived into the first part of the twentieth century.

Photo Above: The Cobourg Harbour side of the completed module illustrates the Cobourg Peterborough & Marmora Railway & Mining Company facilities at the harbour in the late 1870s. Dominating the area at the time was the 1869 built raised ore transfer trestle along the northern esplanade. The original white Cobourg & Peterborough station was in use to the north. A roundhouse was located to the west (left) of the station. A number of smaller buildings occupied various sites in the general area of the railway facilities

Many local residents are aware of my interest in the C&P and at the annual Cobourg Model Train Show I am frequently approached by people wishing to share information and anecdotes with me. At times they approach me with a question anticipating that I might provide an answer to a question relating to the C&P or other local railways. To provide a tangible setting for discussion and information sharing I decided to construct a portable HO layout based upon aspects of the C&P as it appeared in the mid to late nineteenth

century. To this end my accumulated resources assisted in developing a concept that led to a functional layout design.

Consideration of several noteworthy features of the C&P (and its successor) determined that two prominent physical aspects of the railway became essential for inclusion in the layout. One feature was the C&P's more than two mile long trestle and bridge complex spanning Rice Lake. The second feature was the raised trestle at Cobourg Harbour built in the late 1860s by the successor company, the Cobourg, Peterborough & Marmora Railway & Mining Company. This latter enterprise transported iron ore from Marmora, Ontario, by rail to Trent Narrows, by barge to Harwood on Rice Lake and by rail once again to Cobourg harbour for transhipment by lake vessels to U.S. ports.

Including these two highlights and considering the restrictions imposed by transporting a model layout that could be set up by myself, a 4x6 foot portable layout with a divider to create two 2x6 foot modules evolved. This design enables me to transport the two modules stacked one on top

Photo Below: The 13,000 foot long Rice Lake Bridge between Harwood and Hiawatha was the major feature of the 1850s built Cobourg & Peterborough Railway. For the module, only two sections of the 31 sections were modelled to provide an illustration of what was once built across the lake. To the left of the scene an era train is approaching the Harwood Station, a model of the original. To the right is a representation of the Hiawatha Station, original appearance unknown. In the background on the shore one of the two saw mills at Harwood is depicted.



of the other in my vehicle and leave room for other items such as structures, scenery and rolling stock in protective carrying containers. The layout design allows for one side to represent a segment of the 1856 Rice Lake Bridge complex while the opposite side represents a portion of Cobourg's late 1860s harbour front. A view block on which illustrate pertinent historical images and explanatory text visually separates the two modules.

The show purpose dictated a track plan that provided for continuous running. An oval track plan was developed which included representative spur tracks to create a simulated atmosphere for train operation. The over all size of the layout necessitated sharp curves limiting the type of rolling stock. As the original C&P was one of Ontario and Canada's earliest railways this was not a model problem as motive power would be limited to 4-4-0 locomotives and 28 to 30 foot freight cars such as platform (flat) and box cars, and even shorter wooden ore cars. Forty-five and fifty foot long passenger cars were typical of the era. (More about rolling stock in a future installment.) One passenger train and one freight train on the layout, but not operating simultaneously, would represent the

railway activity. To limit handling of rolling stock during show operation a single passing track was included where one train could be held while the other operated. This simplified electrical wiring.

With these multiple considerations, I developed an oval track plan with a single passing track inside the oval on one side. One module represents the harbour terminus at Cobourg of both the original C&P and the later CP&MR&MCo. Two spurs from a single mainline turnout represent CP&MR&MCo operations. From the mainline turnout a second turnout leads one track to a harbour raised trestle spur while the other track leads to a spur on the centre pier of the harbour. The trestle spur allowed ore cars to dump the ore to the ground for manual transfer to the holds of sailing

ships. Labour was inexpensive and plentiful. (An indistinct 1880s image suggests there may have been a moveable funnel arrangement allowing ore to move directly from ore car to ship hold.) The pier track allowed flat cars to bring squared timber and lumber for transfer from the railway to the holds of sailing vessels.

The second module features two bridge sections representing the 31 Burr Truss Bridges that formed a part of the original railway crossing of Rice Lake as built during the 1850s. The remaining portion of Rice Lake bridge consisted of multiple sections of pile trestles at 15 foot intervals. Harwood, on the southern shore of Rice Lake, had a spur leading to a wharf where timber and lumber were loaded onto flat cars. Later this same wharf witnessed the transfer of iron

ore from barges to railway cars by the use of a steam elevator. This feature is represented in one corner of the second module where a switch leads to an ore transfer pier. Beyond the bridge on the north side is the small enclave of Indian Village represented on the module by a small combination station.

Satisfied with the model concept and the track design, construction of the layout began.

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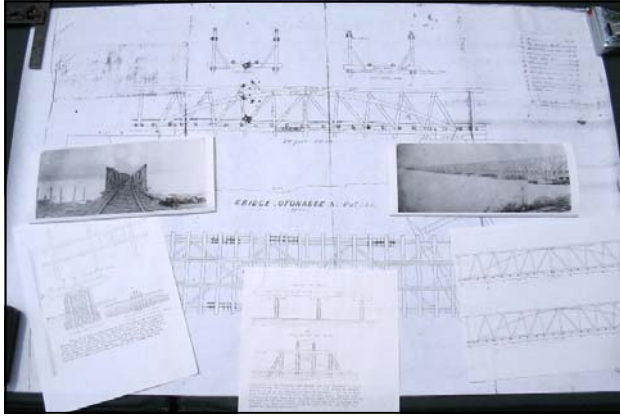
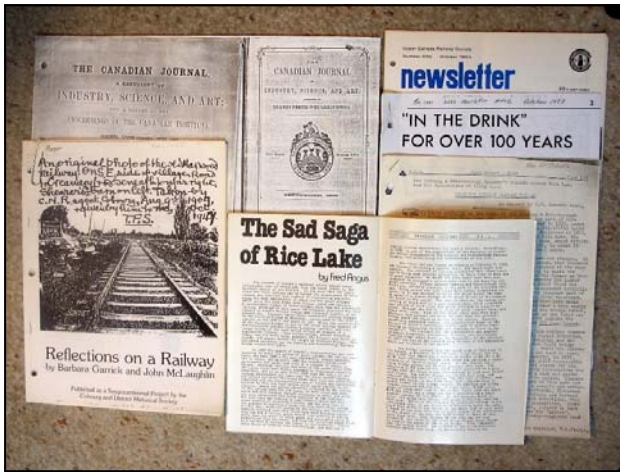


Photo Left: The large drawing on the bottom of this image is a copy of J.H. Dumble's 1857 plan of the Otonabee Bridge in Peterborough. It is identical to the Rice Lake Bridges save the span is 70 feet rather than the RLB's 80 feet. Other bridge and trestle information was garnered from Canadian Rail.

Photo Right: Old images provided additional information regarding the C&PRy. Shown here are related images from the National Archives of Canada, Peterborough Centennial Museum and Archives, Cobourg & District Historical Society and an enlarged image from a local history book



Photo Left: Information regarding the Cobourg & Peterborough Railway was garnered from a variety of sources. Some of these sources are observed in the photo and are articles from Canadian Rail, The Canadian Journal of Industry, Science and Art (19th century publication), Upper Canada Railway Society Newsletters and *Reflections of a Railway*, a locally written and printed booklet of C&PRy items.



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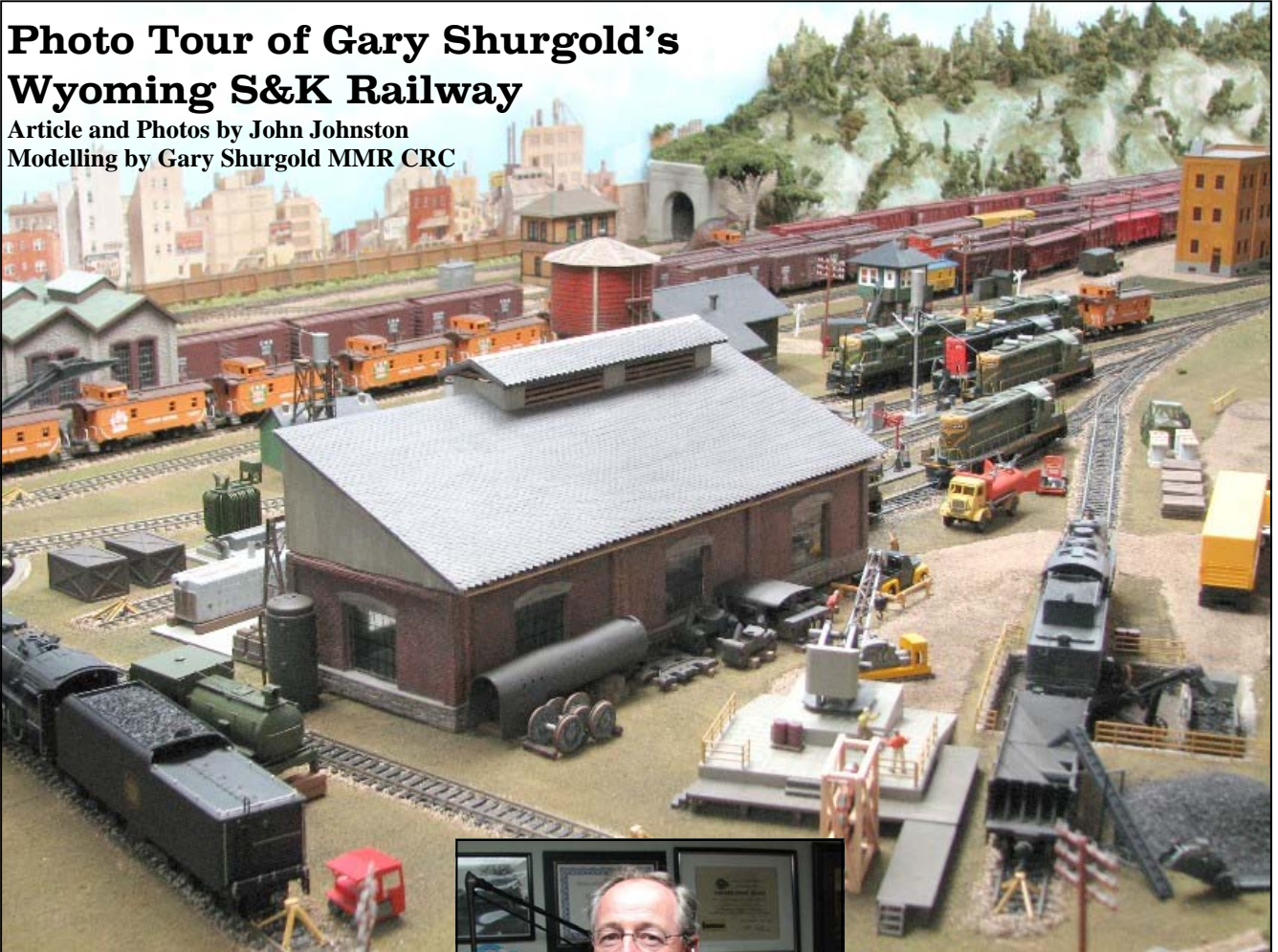
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Photo Tour of Gary Shurgold's Wyoming S&K Railway

Article and Photos by John Johnston
Modelling by Gary Shurgold MMR CRC



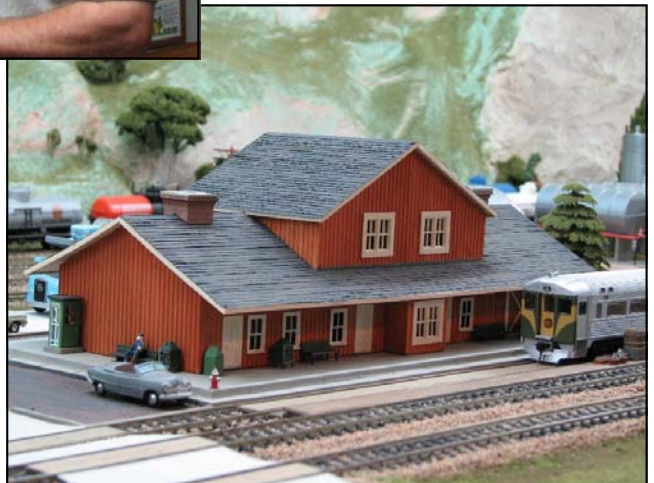
Above: Overview of the engine facilities and yard. Many of the structures are scratchbuilt or kitbashed.

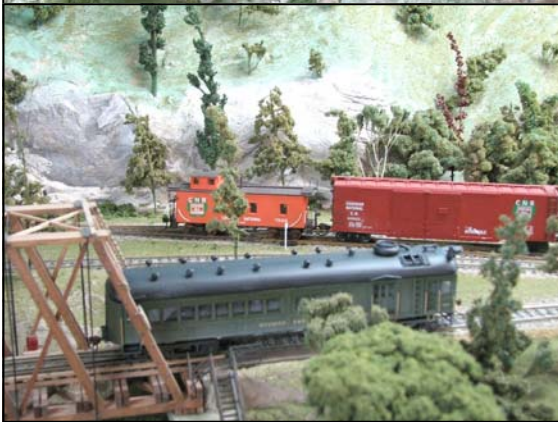
Below: Gary models CNR and here at the engine facility we can see GP9's in both the green and black schemes.



Center: Gary Shurgold MMR CRC

Below: The daily CNR RDC pulls into the scratchbuilt station. Hopefully that shiny new convertible is clear of the tracks when it pulls out.





Gary's layout is a U shaped 22ft on one side, 14ft across, and 19ft down the other side. He models steam and diesel CNR in the transition era. Many of the structures are either kitbashed or scratchbuilt and are named after his numerous model railroad friends. The layout has 5 cabs, block control wiring, and can accommodate up to 10 operators in 5 engineer/switchman teams.

Top Left: Juneco Truss Bridge modified to fit into the abutment arrangement.

Top Right: Town of Hayden. The grain elevator is a Campbell kit which has been modified with scratchbuilt components

Center Left: Eastman's Trestle which was scratchbuilt by Don Eastman and acquired by Gary when Don switched to O Scale.

Bottom Left: Scratchbuilt Gas Electric waiting for a CN freight to clear so it can reach the passenger station at Hayden

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Layout Tours at Pacific Rails 2007 National Convention May 17th to 21st, 2007 University of Victoria Conference Centre

Article and Photos by Ed Warren MMR CRC

One of the most enjoyable parts of a convention for me is the chance to see home layouts in various parts of the country. In this article we will describe some of the layouts you can see at the Pacific Rails 2007 Annual National CARM Convention in Victoria B.C. May 17 to 21, 2007. The drive to each layout is a great lesson in the geography of the host city and it is a chance to make new friendships if you are carpooling. At this year's Pacific Rails 2007 it will also be a chance to see some great scenery in the Victoria area. We are trying to keep costs down for convention registrants, and to make things simpler for the organizers, so all of the layout tours will be self-guided. We will ask that people carpooling gather in front of the conference centre at the tours gathering point where maps will be provided, along with phone numbers if you get lost. The Victoria area is very compact, only about 20 km square, so getting about is quick and easy.

On Friday May 18 the Vancouver Island Model Engineers will open at 1700 to 1930 for a pay as you go BBQ at their picnic area and rides on

their live steam layout. The VIME has been fortunate to partner with the Saanich Historical Artifacts Society, using a section of their 28 acre grounds to lay 3,000 feet of 7-1/2 inch track. They have been very busy with heavy engineering projects installing bridges, abutments and cuts, fills and culverts for a 2,500 foot extension to the railway prior to our visit. The members have an extensive collection of equipment, an impressive shop with a turntable, transfer tables and service tracks. Also on the property the club has 880' of 3-1/2" and 5" gauge track for smaller-scale models. There is a snack bar designed as a station that serves light meals. They will be open for our visit so you can have a burger and ride on the train before going to other layouts in the area. The Historical Artifacts Society has a very interesting collection of pioneer buildings, vehicles and artifacts in a park-like setting, which will also be worth a visit.

Robert Hopkins has a large O Scale railway nearby. It is in a dedicated 30' X 36' room. The bench work is L girder construction and all of the

track is hand laid. The standard gauge is code 125 while the two narrow gauge lines are code 100. The standard gauge follows Canadian (CNR, CPR) practice, although it is a free lance railway and has a connection with the 3' Westside Lumber Co. using hand laid dual gauge trackage. The 3' East Board Top line is a separate railway. The time period is set at 1959, the steam transition era. Steam and first generation diesel rule the rails here.

Walter Thomson's HO Scale layout is built in a single car garage. This is typical Vancouver Island practice as there are few basements here. Most modellers negotiate the use of the garage, as the winters are mild enough that the cars can stay outside. Instead of scraping off snow and ice, you have to clear off the moss occasionally. Walter has built a freelance CPR layout with a logging branch line based on lines in the BC interior in the early 1960's. It is powered by Digitrax Command Control. Motive power is Steam and Diesel to the early 1960's. Walter has done a lot of research to produce accurate models and scenes.

Photo Below Left: Live steam 1 1/2" scale Pacific on the service track at the Vancouver Island Model Engineers layout in Saanich.

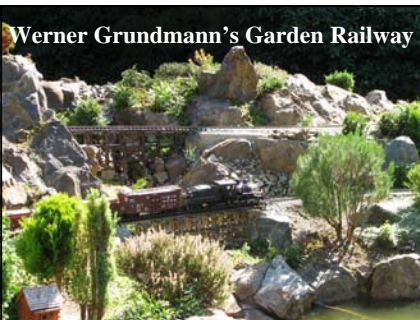


Photo Below Right: Robert Hopkin's O scale CN/CP transition era layout



Werner Grundmann's Northern and Alpine RR is one of the largest and possibly the most ambitious Garden Railroad on the Island. The entire layout was planned on paper before the first piece of track was laid in May 2003. The N&ARR is strictly a narrow-gauge mountainous railroad with some logging operations. The railway has 750 ft. of mainline and a total of 1237 ft. of track over a garden of 2900 sq. ft. Twilight is the most attractive time, since much of the layout is lit by about 200 miniature light bulbs placed in the buildings and throughout the layout. Vancouver Island's mild climate is ideal for garden railways, and there is a very active club here.

Wayne Paulsen has worked for many years on the Cascade Pacific Railroad to bring it to its current highly finished state. It is an O scale freelance model of railroads in the Pacific



Northwest . It runs from tidewater to the Cascade mountains, and features steam and diesel locomotives from the Cascade Pacific, Great Northern and SP&S from the mid-50's to the mid-60's. The fully scened 'L' shaped layout is 44 by 16 feet with a continuous backdrop painted by a professional graphic artist. Handlaid code 125 track is laid on Sugarpine ties with 4 spikes per tie. The railway is operated in a loop or point-to-point for operating nights. There are 14 industries serviced by switchlists during operating sessions. Wayne has a dedicated operating group over every week to help with construction and operation. The group has made a very professional DVD showing the layout in opera-



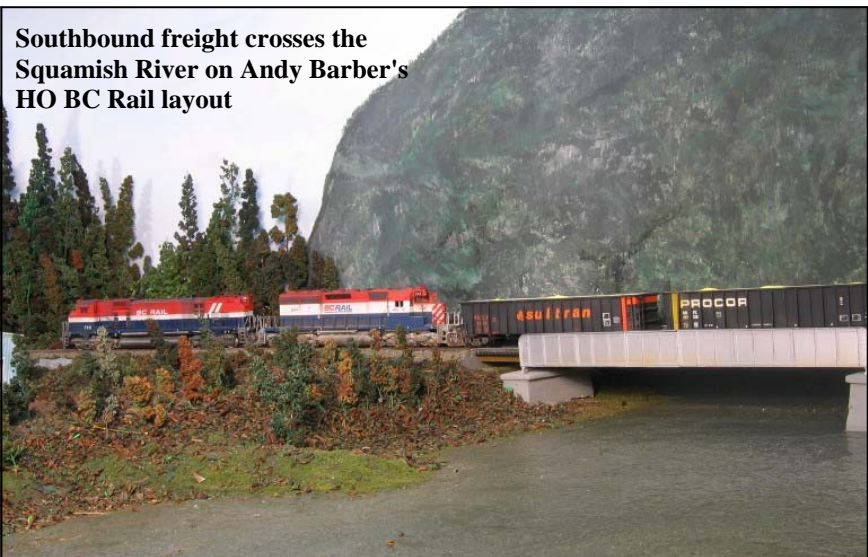
tion, which is available for sale. There are several removable sections that are taken to train shows, and combined with other member's sections to make up an award winning sectional layout.

On Saturday evening, the layout tours are in Cobble Hill and Shawnigan Lake, a short but incredibly scenic drive over the Malahat, a mountain pass that is the only exit from Victoria if you want to head north, up Island. There are several scenic viewpoints at the summit, and the evening light should be perfect for photos. If it

is clear, you will see the volcanic Mt Baker in northern Washington state in the distance.

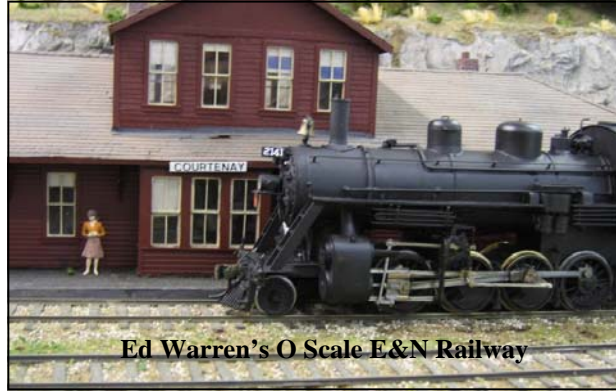
Two of the layouts you will see have been featured recently in Canadian Railway Modeller Magazine. Andy Barber has a highly accurate HO layout of the BC Rail line from North Vancouver to Squamish, a distance of 41 miles. It is a two level layout with highly effective backdrops, and a hidden helix and staging yard.

Peter Gilbertson's layout was also the December 2006 CARM calendar pic-



Southbound freight crosses the Squamish River on Andy Barber's HO BC Rail layout

ture. His HO Durham Sub is a freelance layout based on CPR operations in the Rockies in the late 1960s. This allows him to run first to third generation power in the maroon and grey as well as action red colours. It is a large home layout in a 40 by 40 foot dedicated room, 1700 feet of track and a double track helix serving three levels. The dramatic mountain scenery is 60% complete.



Ed Warren's O Scale E&N Railway

On Monday May 21, we have arranged for layouts in Qualicum Beach / Parksville to be open. This is about 150 km north of Victoria by highway, about 1 1/2 hours drive. If you are returning to Vancouver, you can get the BC ferry from Nanaimo which is nearby, so you won't need to backtrack to Victoria. There is a lot of railway memorabilia to see in the area, such as logging engines in Qualicum Beach, Ladysmith and

Chemainus, and the water tank in Parksville. Almost all of the original E&N stations are intact and restored and in use. Via Rail runs daily service from Victoria to Qualicum Beach and beyond, and we are encouraging people to travel by train. It leaves Victoria that morning at 1000 hr, from the downtown station. (Those wishing to travel by train should book their tickets at www.viarail.ca or 1-800-842-

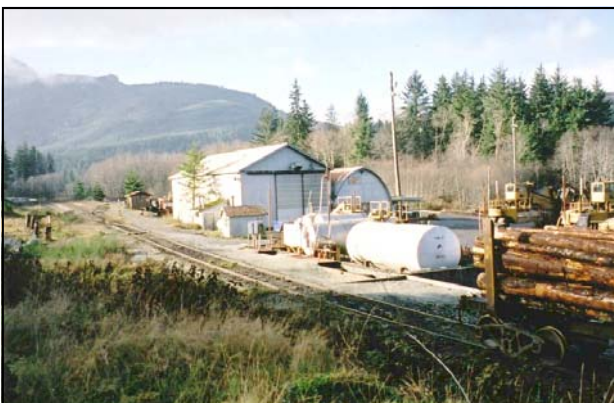
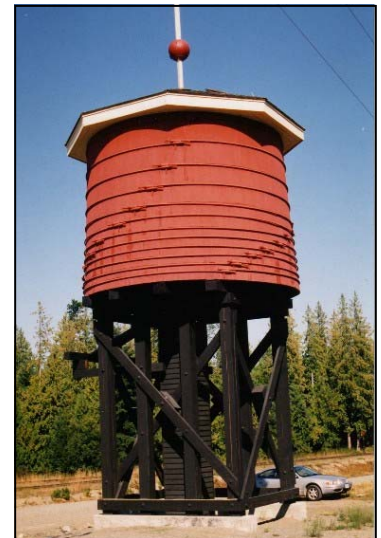
7285). Those arriving in Qualicum Beach by Via train at 1330 will be picked up and escorted to layouts by volunteers of the Mid Island Railway Club in vans. Look for the PR2007 t-shirts on your guides. You will be brought back to the station for the 1600 departure. If you are travelling by car you will be given maps before you leave Victoria on Monday morning.

We currently have four home layouts for you to see; Ed Warren's O scale (P48) Esquimalt and Nanaimo, Mac West's N scale CSX Mountain Sub, Wolf Riedel's HO German State railways, and Marty Phillip's HO Maine Central. We hope to see you in Victoria in May. More convention information is on the CARM website, or contact Ed Warren, chair, at 250-752-4857, or e.a.warren@telus.net.

Photo Right: The E&N Parksville water tank has been restored and is next to the Parksville station, and can be seen while on the Monday layout tour.

Photo Below Right: MacMillan Bloedel 1066 has been preserved under cover next to the Qualicum Beach station, and can be seen during the Monday layout tour.

Photo Below Left: Beaver Cove is a 4 hour drive north of Victoria, and the terminus of the Canfor (Englewood) Logging railway, the last operation of its type in North America. This is the engine servicing facility at the Beaver Cove log sort





Pacific Rails 2007
CARM/CRHA National Convention
 ---Registration Form---
 May 17th to 21st, 2007
 University of Victoria Conference Centre
 Victoria, British Columbia, Canada



Part 1) General Information:

First Name: _____ Last Name: _____
 Address: _____
 City: _____ Province/State: _____
 Postal/Zip Code: _____ Country: _____
 Phone Number: () _____ E-mail: _____

Note: Dates listed at right are based on date received by registrar.
 Cross out all prices that do not apply

Part 2) Primary Registrant (check all that apply):

	Before March 15 th	On or After March 15 th
Tag Name (first & last): _____		
I am a member of <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>	\$ 75	\$ 90
CARM CRHA CPSig CNSig MIRC VMRC WCRA WVIHS		
I am not a member of any of the above groups <input type="checkbox"/>	\$ 85	\$ 100

Part 3) Additional Registrants (must be immediate family & same address):

Tag Name (first & last): _____	\$ 7	\$ 7
Tag Name (first & last): _____	\$ 7	\$ 7
Tag Name (first & last): _____	\$ 7	\$ 7

Part 4) Sunday Banquet Tickets: \$48 each x _____ (quantity) = \$ _____

Part 5) Registering at the convention surcharge: XXXX \$ 10

Part 6) Registration Total (add all dollar values as required, parts 2 to 5): \$ _____ \$ _____

If paying in Canadian Dollars skip Part 7.

Part 7) Payment in American Dollars (Part 6 x 0.90): \$ _____ \$ _____

Part 8) Display Items (list items on the back of this form)

I am planning on bringing models, photographs or other items for display.

Part 9) Submit Payment:

Submit your cheque/money order in Canadian Dollars from Part 6 **or** in American Dollars from Part 7 made payable to "Pacific Rails 2007" and mail it to the address below. Registrations must be received no later than May 10th, 2007. After May 10th, 2007 registrations will only be accepted at the convention:

Pacific Rails 2007
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Canada



Pacific Rails 2007
CARM/CRHA National Convention
 ---Fine Print---
 May 17th to 21st, 2007
 University of Victoria Conference Centre
 Victoria, British Columbia, Canada



Registration Form Details:

Please fill out all information located on the Registration Form in **Part 1**. If you have an e-mail address please fill in this information as it is our preferred method of contact when sending out registration confirmations, convention updates and other information. If an e-mail address is not included or is invalid your information will be mailed by Canada Post and it will take longer for you to receive the information in this manner.

In **Part 2** fill in the name, first and last that you would like to see on your name badge. Also check any of the boxes that apply to your membership(s) in any of the groups that are involved with the convention. If you are not a member of at least one of these groups check off the box for that choice. Cross off any fees that do not apply to your registration.

In **Part 3** add any additional registrations for other members of your immediate family that live at the same address and are attending the convention.

The Sunday Banquet is an extra fare event with a limited capacity of 325 people on a first booked basis. The cost is \$48 per person for all that will be attending. Complete this section, **Part 4**, if you or any of the people listed on your registration form is planning to attend the banquet.

A surcharge for registering at the convention will be applied as listed on the registration form.

In **Part 5** add up the totals for Parts 2, 3, 4 and 5. If you are paying in Canadian funds skip **Part 7**. If you are paying in American funds complete **Part 7**.

Fill in **Part 8** if you are planning on bringing any models, photographs or other items for display during the convention. The convention committee will provide a secure area but they do need an idea of how many items are going to be displayed so that the required space is made available. Please provide a short description of the size, weight or other information about the items that you will be bringing on the back of the registration form or on an attached sheet.

Finally complete **Part 9** by submitting your cheque or money order in Canadian or American funds made payable to "Pacific Rails 2007" along with the registration form and any other information to the address located on the registration form. We have found that in the past couriers are not really any faster than regular postal services since no one is at home during the day to accept a courier delivery. Do make sure that you give your registration plenty of time to reach the registration address as all dates listed for registrations, cancellations or other mailings are based on the date that the mail is received.

Other Extra Fare Events:

There will be extra fare items for features such as tours, food events and other possibilities. People who are already registered for the convention will be notified of these items prior to the information being released in mass so that the early registrants will have the first chance at booking activities where numbers must be limited.

Cancellation Policy:

Cancellations must be received at the Hamilton address by May 1st, 2007 for full refund, by May 10th, 2007 for full refund minus all food costs. No refunds after May 10th, 2007. Any refunds will be issued after the convention.

Latest Information:

For up to date and more information check our website at www.caorm.org and follow the links to the convention site. For information or inquires about registration please e-mail David King at membership@caorm.org or send postal mail to the address located on the registration form.

COMING EVENTS



BRITISH COLUMBIA, VICTORIA, MAY 17-21 4TH ANNUAL CARM CONVENTION

Ontario, Kitchener-Waterloo, March 31, 24th Annual Doubleheaders Self Guided Layout Tour. 30+ Layouts. Tickets and Maps, Hespeler Arena, 640 Ellis Rd. W., Cambridge. 9:00 am to 9:00 pm. \$5 for every person. For information call 519-632-8604

British Columbia, Prince George, April 6, 7: CROSSROADS EXPRESS 2007, 7th Division PNR, NMRA, Interior Spring Meet, Prince George Civic Centre, 808 Civic Plaza. \$3 rebate to NMRA and CARM members. Contact Bill Hood, Registrar, 209 - 1601 Ingledew St., Prince George, BC V2L 1K6. Phone (250) 563-4779 E-mail: bhoodpg@shaw.ca

Ontario, Brantford, April 14: 2nd Annual Brantford model railroad tour. 9 AM to 5 PM. Registration at Balfour United Church, 62 Balfour Street. Admission \$5 adults, children under 12 free accompanied by an adult. Proceeds to the school breakfast fund. For further information contact Roger Chrysler, (519) 753-9413

New Brunswick, Riverview, April 14: Coverdale Model Railroad Show, 9 AM till 4 PM, Coverdale Recreation Centre, Runnymede Rd., off Hillsborough Rd, Rte 114, Adults - \$4 Children 3-12 years - \$1, Operating Layouts all scales, Sales Tables, Dealers

Ontario, Lindsay, April 14, 15: 33rd Annual Lindsay Model Railway Show 2007 featuring historical displays; also planes, boats, and automobiles. Sat. 10am-5pm; Sun. 10am-4:30pm, Victoria Park Armoury, 210 Kent St. W. Admission: Adults \$5.00, Seniors & Students (with ID) \$4.00, Children 6-12 \$2.00, Under 6 Free. Contacts: Don McClellan (705) 454-2746; Larry Murphy (705) 328-1486 E-mail: ldmrclub@hotmail.com

British Columbia, Nanaimo, April 15: 23rd Annual Mid Vancouver Island Railway Show Sunday April 15th 2007. Beban Park Rec. Center, 2300 Bowen Road. 10:00 am to 4:00 pm. 7th Division P.N.R. N.M.R.A. and C.A.R.M. Admission \$3.00 members, \$5.00 non member, \$10.00 family, \$4.00 seniors. Information, contact Rick Lord, 8841 Faber Road, Port Alberni B.C., Canada V9Y-9B8. phone 1-250-724-4205 E-mail: rblord@telus.net

Ontario, Schomberg, April 21: Second Ontario Narrow Gauge Show, 10 am to 4 pm, Schomberg Community Hall, featuring layouts, dealer, manufacturers, model & photography contest, workshops & demonstrations. Lunch counter available. Admission: \$10 advance, \$15 at the door. Contact Christopher Creighton for more info at 905-939-0694. E-mail: narrowgaugeontario@hotmail.com

Saskatchewan, Moose Jaw, April 21, 22: Thunder Creek Model Railroad Club Model Train Show April 21 - 22, 2007. 9 am to 5 pm. Adults: \$7.25; Seniors: \$6.25; Students: \$5.25; Children: \$2.00 Family \$16.00 Moose Jaw Western Development Museum, 50 Diefenbaker Drive, S6J 1L9. Info - Jackie Hall, moosejaw@wdm.ca or (306) 693-5989

Ontario, Iroquois, April 21, 22: Model Railroad Show and Sale, Seaway District High School, Hwy 401 to exit #738, south to Hwy 2, turn left (east) on Hwy 2 for 3 blocks. High School is on south side. Information contact Jason Edge at cuttingedge@ripnet.com or 613-657-1392

May 5, 6: Innerkip, ON, Hank Duivenbode S Scale Open House: RR#2 Innerkip, from 11 am to 5 pm. No fee. Contact Hank for directions 519-469-3695

Ontario, Stayner, May 5: Nottawasaga Model Railroad Club 13th Annual Model Railroad Auction. St. Patrick's Parish Hall, corner of Cedar and William, off Airport Road, behind the IGA, Open at 8:30AM for consignments; preview at 11:30, Auction from 12 Noon on. Admission \$4.00. Coffee Kitchen. Contact Dolf Roelofsen (705) 445-7317 or Bill Payne (705) 429-2762. E-mail contact kmdouglas@sympatico.ca

Ontario, Aberfoyle, May 5, 6, 12 & 13: Aberfoyle Junction Model Railway Open House. Large O Scale Club. #128 Brock Road, 1.5 km north of Hwy 401, Exit #299. Open from 10 am to 4:30 pm. Adults \$7, Students/Seniors \$5. Children \$4. Info: Craig Webb, 257 Broadway Ave, Hamilton, ON, L8S 2W7, 905-527-5474

Ontario, St. Thomas, May 13: Railway Nostalgia Day May 13, 2007. 10 am to 4 pm. Admission: \$4.00; Under 12 free. Elgin County Railway Museum, Wellington St., Info - 519-637-6284 email:promotions@ecrm5700.org

Ontario, Midland, May 26/27: The Midland & District Railroad Club 19th Annual Model Railroad Show. May 26 10:00am-5:00pm. May 27 10:00am-4:00pm. At the Midland Sports & Recreation Complex, King Street. Admission: Family \$10, Adults \$5, Seniors \$4, Children \$2. 17+ operating layouts. The biggest show north of Toronto. 60 tables of vendors, door prizes, fun for the whole family.

Ontario, St. Thomas, August 26: Railway Heritage Day 10 am to 4 pm. Admission: \$4.00; Under 12 free. Elgin County Railway Museum, Wellington St., St. Thomas ON Info - 519-637-6284 email: promotions@ecrm5700.org

Maine, Portland, August 29-September 2: 27th Annual Narrow Gauge Convention, www.27thnarrowgaugeconvention.net%20

Ontario, Holland Centre, September 22: The 2nd Annual "Day at the Clinics". 10am to 5pm. Holland Centre (on Hwy 10) then east 5 minutes on county #30 at Participation Lodge. Featuring operating layouts, displays, Clinics, vendors/exhibitors. Admission - \$5.00 Children under 6 free. We are always looking for people to give clinics. Further information/table request, contact Randy O'Hara at 519 371 1998 attheclinics@yahoo.ca

Nova Scotia, Truro, October 13: Truro Model Railroaders Association presents their 30th ANNIVERSARY (1977 to 2007) FALL TRAIN SHOW at the Nova Scotia Community, Time 9:00am - 4:00 pm, College, Arthur Street. Admission: Adults \$5:00 • Students \$3:00 • Children under 12 Free if accompanied by parents. You will see Operating Layouts in Z---N---HO scale; Railroadng Sales and a variety of Display Tables. Contact Persons Tex Corning 893 1448 or Howard MacLellen 897 0642, Email: jcorning@eastlink.ca or pai@ns.sympatico.ca

Ontario, Harriston, October 27: Annual "BOOMERS" Auction of model railroad equipment and materials at the Harriston Community Centre. Admission \$5.00 (includes door prize draws). Tables available at 8:00am. Auction starts at 10:00am. For additional Information contact Harold G. Jones, 16 Conroy Cres, Guelph, ON N1G 2V6, 519-821-2454. Email: hgjones@golden.net.

Ontario, Whitby, November 17/18: Pine Ridge Railroader Model Train Show, Saturday 10 am - 5 pm and Sunday 10 am - 4 pm. Father Leo J. Austin School, 1020 Dryden Boulevard, Whitby (near Anderson, south of Tauntan) Adults \$5.00, Children under 14 - \$2.00, Children under 5 - FREE. www.trainweb.org/prc



Ted Rafuse's Cobourg & Peterborough Railway

Above: Shay, the author's seven-year old grandson to the right of the divider, operates the modules during the public display at the Cobourg Model Train show. This allows the author to talk with the public and distribute a pamphlet with historical information without having to worry about the running of the trains. The layout's popularity is reflected in this image portraying a variety of people of all ages and both sexes.

Below Right: Walter Thompson's HO freelance CPR and logging railroad

Below Left: Wayne Paulsen's O Scale Cascade Pacific Railway at Lakeview station.

