



THE "CANADIAN"

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WINTER 2016 ISSUE #54

ALL CARM MEMBERSHIPS EXTENDED FOR ONE YEAR FREE



a quarterly publication of the "Canadian Association of Railway Modellers"



THE CANADIAN ASSOCIATION OF RAILWAY MODELLERS

Founded October 15, 2003

Founding Members: John Johnston, Peter Moffett,
David King, Lex Parker

DIRECTORS

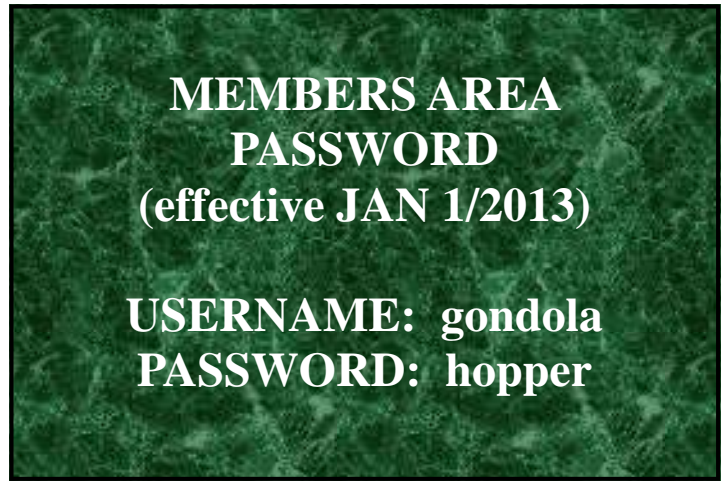
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FRONT COVER UPPER PHOTO BY GERALD HARPER: CP Rail Xmas Train with EMD GP20C-ECO at Lambton Yard on November 30, 2015.

FRONT COVER LOWER PHOTO BY GERALD HARPER: Lit up cars on CP Rail Xmas Train at Lambton Yard, November 30, 2015.



observation platform john johnston: editor

POSSIBLE MEET AT COPETOWN TRAIN SHOW ON SATURDAY, MARCH 5TH

I will be trying to put together a CARM Meet to be held on Saturday, March 5th. This will involve a number of clinics in the afternoon and a slide show in the evening with both lunch and dinner being provided by the Copetown Lions Club. Over the next number of weeks I will be corresponding with Chapters in the Southern Ontario Area to determine how much interest there is in moving forward.

If we do get the Meet set up you will be receiving a separate notification both in the mail and by email. Because there are food costs involved with the setup we will undoubtedly be asking you to let us know in advance if you plan to attend. Look for more information to come out on this event in late January or early February.

Also, if you are a regular attendee at the Copetown Show, check out the new date on the flyer to the right. We are trying to move away from some of the bad weather that has plagued us in recent years.

OPERATIONS ON THE GRAND TRUNK SOUTHERN

I have continued to explore all of the options for operations on the Grand Trunk Southern. I have now held two operating sessions using the Car Card System. Overall I was pleased with how this tried and true method moved cars prototypically. On the downside, my trains have up to 25 cars and that is a fistful of car cards to be carrying in your hands and trying to sort when you reach a town where cars have to be switched. There is also the issue of aging eyes trying to read car numbers on N Scale cars. The question is do the pro's outweigh the con's. I'm still approaching this with an open mind and looking at other options.

I recently attended the HOMES Club here in Hamilton for an operating session with both of my grandsons in tow. The Club was hosting those of us who had opened our layouts during their recent Layout Tour Event. The Club uses JMRI software to develop switchlists. Rather than a handful of cards we had a single sheet of paper with clear instructions on what actions our train would have to undertake. Discussions with Club Members assured me that the

issue of staging had been dealt with in the JMRI Program. This had been the downfall of Ship It software which I had tried in the past. I was impressed with the results as were my grandsons. Lots of pro's but also a few downsides. Chief among the downsides were that you still had to be able to read N Scale car numbers. Difficult on clean cars, tougher still once the cars are weathered. It also requires a commitment to learning the software and then maintaining the system once you have it set up. For example, after a session you have to ensure that all cars were moved appropriately and are where the computer thinks they are, otherwise the next set of switchlists will be inaccurate. Downsides notwithstanding, I like the prototypical nature of the switchlists and this method is still being actively considered.

Thirdly, I have been looking long and hard at the Tab On Car System. This method was pioneered by one of the giants of model railroading history, John Allen, on his Gorre and Daphetid Railroad. Those of us of a certain age can remember growing up looking at photographs of John's masterpiece of a model railroad in the pages of Model Railroader. His method of using cardboard strips was taken a step further by the likes of Ed Raven-



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COPETOWN TRAIN SHOW

NEW DATE

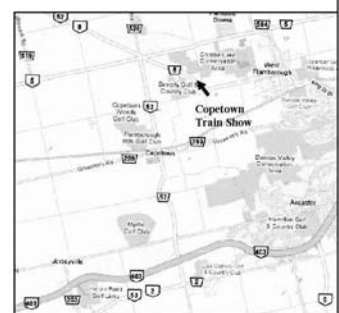
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10:00 am — 3:00 pm

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scroft we drilled a small hole in the top of his cars and used coloured tacks.

This method really fell into the background until it was resurrected by a Canadian N scale modeler, Mark Dance of Vancouver. Mark's N Scale Columbia and Western has recently received much acclaim in the modelling press such as Model Railroader, Model Railroad Craftsman, and Great Model Railroad.

Mark uses strips of C channel styrene colour coded for his various town with labelling on the styrene for the various industries within that town. His tabs are small and unobtrusive, though they do stand out when you see them in photographs. A quick web search showed me that a number of other significant N Scale layouts had chosen to follow Mark's lead and adopted this Tab on Car method. The biggest advantage is that you no longer need to read those pesky car numbers, you just follow the instructions on the car tab. It greatly simplifies yard operations. The operator looks at his cars rather than the paperwork. The obvious downside is appearance. You have this unprototypical tab on the top of your car.

I have corresponded with Mark and two of the other layout owners who have adopted this method. All of them indicate having similar concerns when they adopted this method and all had surveyed their operators to get feedback. Overwhelmingly operators say that once the session starts they don't even think about the appearance of the tabs and that overall operations are much more fun.

Those last comments are what have me 90% of the way to changing to this method of operations. I'm building this layout for fun, not as a museum piece.

I won't be scrapping my car card system yet, and I have not discarded the thought of going to JMRI, but I am going to invest the small amount it takes to get the styrene, paint it up, add the destination labels, put them on my cars, run an operating session, and test out this system to see if it is the one for me and the group of operators who run the Grand Trunk Southern.

CHANGES TO CARM

The Executive Board led by new Chair Gerald Harper are making a concerted effort to give you greater value for your CARM Membership and to build the CARM Membership back up.

As a first step in adding value you have received with this *Canadian* a 2016 CARM Calendar put together by the efforts of Walter Reid. Thanks to Walter for his work.

You will also find mentioned in a couple of places in this issue, as well as on the front page headline, that all CARM Memberships are being extended for one year free. This will provide the Executive with some breathing room to implement some of the changes, particularly with the website, that they have been discussing.

You will also read about some changes to Membership dues. These take effect January 1, and as we move forward into 2016, we will be talking more about these changes.

JOHN JOHNSTON: EDITOR



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CHANGES COMING TO CARM

The Executive Board held a meeting in late September and reviewed all of the submissions that were sent in to *The Canadian* by Members as well as taking into account their own knowledge of what has been happening with CARM. All of the Executive Members expressed their appreciation to those who had taken the time to put their thoughts to paper and share them.

After a lengthy discussion, a number of conclusions were reached.

1: CARM has to begin a migration towards a larger Internet/Social Media presence while at the same time maintaining a commitment to those Members who receive information in a paper format.

2: CARM has to address the issue of the value it is delivering to its Members. The first tangible evidence of increasing this value will be the re-introduction of the CARM Calendar.

3: CARM has to review the cost of Membership and look at introducing new low cost options.

4: CARM has to address the fact that Members outside of Ontario feel that they have no contact with the National Organization.

In reaching these conclusions the Executive Board also restated its commitment to the founding goals of CARM which are:

to inspire.

to foster fellowship among railway modellers throughout Canada.

to provide a forum for the exchange of ideas and skills between railway modellers.

to promote the hobby of railway modelling in Canada.

to be inclusive of all scales and interests.

to work in co-operation with other organizations.

The Executive Board committed itself to a number of immediate steps and other longer range actions. Amongst these were:

1: Re-introduction of the Calendar, which is to be shipped with this issue of *The Canadian*.

2: Making an effort to create a mini-convention in 2016 if a full convention site is not available. Copetown in March

2016 is being considered, as is the Montreal/Delson area in August 2016.

3: Revamping the Website to make it more of a value added asset for Members.

4: Continuing a dialogue with Members and Chapters outside of Ontario to see how we can benefit them more fully.

5: Creating new membership opportunities.

NEW MEMBERSHIP OPPORTUNITIES

The Executive has reviewed the cost of providing services for each class of Membership, and effective January 1, 2016 is putting the following Membership Classes into effect. It is important to note that each of these classes represent Full Membership within CARM. The difference between them is the manner in which services are delivered.

1: Print Membership: Cost \$36. This class of Membership will receive a mailed copy of a printed Canadian, as well as a Calendar. The cost of this Membership represents the cost of printing and mailing the Canadian, and printing and mailing the Calendar.

2: Internet Membership Type 1: Cost \$10. This class of Membership will receive an Internet copy of the Canadian. The cost of this Membership represents the cost of printing and mailing the Calendar. Anyone who has paid this membership in advance will have their expiry date amended to reflect the new fee structure.

3: Internet Membership Type 2: Cost FREE. This class of Membership will receive an Internet copy of the Canadian and will not receive a Calendar.

ONE YEAR FREE MEMBERSHIP FOR ALL CURRENT CARM MEMBERS

The Executive in considering the impact that these changes would have on the Membership made the decision that all current Memberships would be extended for one year at no cost to Members. This membership cost hiatus will provide time for the Executive to implement all of the changes which they have under consideration, particularly with respect to the website.



CHAIRMAN'S REPORT FROM GERALD HARPER

The more I talk with CARM members the more I am reminded that the vast majority of the members have joined for the fellowship and networking that membership brings with it. Chapters are a vitally important part of that networking capacity. Conventions, albeit less frequent, are also great events to get together with your fellow modellers and learn about their experiences, share yours, listen to clinicians, visit other layouts and visit prototypes.

Probably the most important aspect is visiting with other modellers and seeing their layouts whether it is for an operating session or an open house visit. You see how they have done it, how they frankly discuss their mistakes and describe their difficulties. Very often they are looking for help from you as much as you think you are the learner. Make the most of such events and if you don't know how to find out about such events then join CARM, if you are not already a member, and watch the notices section in *The Canadian* and on the newly to be revamped website. We plan to focus much more on informing members of what is going on and also how you can organise such an open house or tour for fellow modellers in your area.

On Saturday November 7th I was able to attend the HOMES club layouts open house along with my friend Harold Kemp. This Hamilton based model railroading group hosts an open house tour of various of their members layouts one day a year and provides a program with maps and descriptions for the modest fee of \$5.00. Over the years it has become very popular and people attend from all over southern Ontario and northern New York State. Of the 20 plus layouts on offer I was only able to get to see 8 in a full day of driving and visiting in which I clocked up 272 kilometres. That I only visited eight was because I enjoyed each one so much that I spent a lot of time there. I visited narrow gauge On3, On30, standard gauge N and HO layouts as well as a street car and urban layout. I thoroughly enjoyed all of them and had very interesting discussions with all of the owners and operators, such that I got the feeling that the owners were enjoying the open house as much as the visitors. My sincere thanks to the HOMES Club members who organised this event for an enjoyable day. **On the following pages I have shared with you some of the photographs that I took that day.**

While the HOMES club as such is not a CARM member many of the individual layout hosts are

members as were also the visitors. To the CARM members in southern Ontario who did not attend I suggest you should watch for the event date posting next year and make it a priority. In a similar vein members from the Ottawa Chapter recently travelled to Montreal to tour several impressive layouts there. The Toronto Chapter is investigating the possibility of hosting a similar event to the HOMES tour in the greater Toronto area. Vancouver Island members have also been connecting for get togethers for operating sessions up and down the island.

If you would like to organise such a tour in your area or would like information on how to go about trying to organise such an event or offer your layout for an open house contact your CARM Regional representative and they can help. Alternatively if you have been hosting such events and would like more publicity for them let us know so we can advertise it on the new re-vitalised CARM website and include it in the monthly WHATS ON that will be starting up in 2016.

I also want to share with you an important decision that the Executive Board reached at its last meeting. We were discussing how to promote CARM, while at the same time implement the changes that we had been discussing. That raised the issue of current Members and how to retain them while change is occurring. We also considered our past efforts at increasing Membership and the cost of doing that. It was decided that our best sales people were our current Members and we should focus our attention on retaining current Members and supporting them to help us gain new Members. To that end, the Executive Board has decided to provide **One Year's Free Membership** to all existing CARM Members at the Membership Level that they are currently paying for. That means that if your expiry date was December 31, 2015, it is now December 31, 2016. We are hopeful that in the coming year we will be able to make the changes that we have talked about and you will not only feel comfortable with your membership but that you will also seek out others to join us.

Meantime winter is upon us so that means more modelling and operating time. Best wishes for Christmas and the New Year.

GERALD

Gerald Harper's Photos of his Homes Club Layout Tour Visits



PHOTO ABOVE: Rick Davis' 11X26 freelanced HO layout.

PHOTO BELOW: Building under construction on David King's On30 Colorado & Wyoming Railway



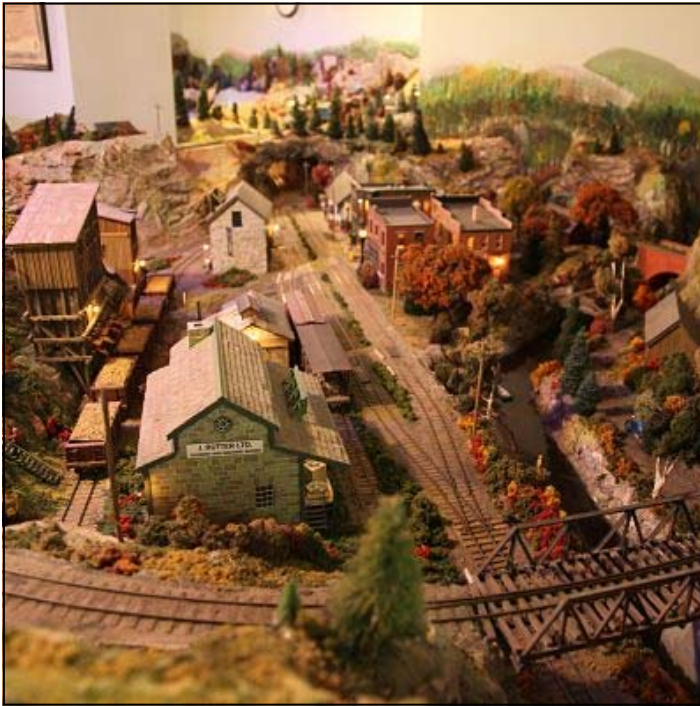


PHOTO ABOVE LEFT: Craig Webb's On3 Algoma River Railway

PHOTO ABOVE RIGHT: Engine terminal on Ross Oddi's freelanced HO layout

PHOTO RIGHT: Hopper unloading facility on Steve Tuff's HO shelf layout.

PHOTO BELOW: Tank car unloading facility on Ron Tuff's HO British Columbia Railway



MID WESTERN ONTARIO CHAPTER:

The Ontario Midwestern Chapter held its Annual Fall Meeting on October 18, 2015, at the Marine & Rail Museum in Owen Sound, Ontario. In attendance were Graydon Hancock, Steve Hoshel, Judith King, Paul Korhonen, Mike Pickup, Randy Schnarr and guests Peter Bowers and Torben Hawksbridge. Regrets were sent by Walt Annett, Lloyd Koch, Robert McKinlay, Dolf Roelofsen, Peter Stamford, and Jade Wassink.

Steve opened with a welcome to all, and introduced the guests. Those present approved the Agenda, Prior Meeting Minutes, Financial Statement, Membership List (12), and Show Report. Paul Korhonen presented our chapter with a cheque of \$139 for our support of the Grey Central Show.

Promoting the Hobby: Recent Switching Challenge activities include the Mount Forest Fireworks Festival in August, Paisley Steam Show in August, the Grey Central Show in September and the Brampton Train Show in October. We also made a list of events we felt we should attend. October 31: Boomer Auction November 1: Kitch-

ener Train Show November 7: Hamilton Layout Tour February 14: Barrie Train Show February 15: Bruce County Museum Heritage Day Show March 13 or 20: CARM MWO Spring meeting March 13 or 20: Copetown Model Train Show.

Project Challenge/ Items of interest:

Paul Korhonen brought photos of latest developments on his layout.

Graydon Hancock showed poster sized pictures of scenes on his layout.

Steve Hoshel showed a Kadee hopper which he decaled. His first. Well done.

Peter Bowers showed several gas electric rail cars from his collection.

Mike Pickup showed pictures of his railway expansion project. Growing well.

Judith King showed the plans of a barn that she is building. Quite a challenge.

Randy Schnarr introduced ideas for a railway display/layout at the Bruce County Museum.

Torben Hawksbridge introduced his N-Scale layout in the museum for young visitors to operate.

Action: by: All Members. Take the challenge for our



next meeting. Pick a category that fits into your layout long term plan and tell a 3-D story, creatively. **Have fun!**

Other Business:

Gord Baverstock Recognition: Paul moved to recognize the contributions of Gord Baverstock as the long celebrated auctioneer of the Boomers Auction with a certificate of achievement from CARM. It was agreed that the Chapter will subsidize 50% of the purchase of a Chapter shirt for each member. Steve to investigate sourcing. Judith brought two books which were surplus to her collection. Tickets were drawn to determine who would receive them. Mike received "Steam over Stratford" and Peter received "Hamilton's Other Railway". Steve won the door prize, a RailCrew Uncoupler. It was agreed to donate \$100 to the Marine Rail Museum.

Art Ball: Passed away quietly at his home on September 1, 2005. Art was a charter member of the Ontario Mid-Western chapter of CARM. He was highly respected in the model railroad community.

Next Meeting: March 13 (or 20), 2016. Date to be alternate to Copetown Show. Location to be announced.

GOLDEN HORSESHOE CHAPTER:

The Golden Horseshoe Chapter met on Saturday November 21, 2015, at the Canadian Legion in Dundas, Ontario. In attendance were Tony Czerneda, Tom Allan, David King, John Johnston, John Bellsmith, Mert Hambly, Mark Magee, Jackson Magee, Gerry Wilkinson, and Guests; Rob Essery and Don Wesley, London Chapter, Brandon Bayer, Ian and Joan McIntosh, Bernard Hellen, Toronto Chapter.

Tony called the meeting to order at 1210 pm and welcomed everyone to the Golden Horseshoe meeting. Tony explained that we have setup workshops and suggested those present get involved. Suggestions for future workshops are welcomed and Tony invited those present to send them in. Our next meeting will be in April and this will be our annual meeting with election of the Executive for this Chapter. Tony urged those present to consider putting their name forward to help run the chapter. Just submit your name to Tom and what position you would like to take on. Tony also told members about our Facebook Page.

Tony introduced John Johnston from our National Executive. John told the group that some changes would be forth coming and the changes will be announced shortly. These changes are intended to make CARM better. He also told the group that a calendar will be included with the next issue of *The Canadian*.

Tom reported that we have \$1093.33 in the bank plus \$5.00 member's fee plus \$45.48 petty cash totalling \$1143.81. Being as we get this room for free, Tony and I buy about \$75 to \$100 worth of door prizes. We try to buy items that go with the clinic being presented.

John Johnston was then reintroduced to do a clinic on Operations. A handout was given out to all present. John will do the presentation today and has picked three different dates in February to come to his home to get some operating experience. , John invited those present to come up and see his layout today after his presentation but it won't be an operating session today as there are too many members present for the size of his layout.



PHOTO ABOVE: John Johnston giving clinic on operations.

Using a PowerPoint presentation John gave an overview of data you need to collect so that you can setup your railroad. He also gave an overview of Train Scheduling and Car Forwarding. John talked about what a realistic operation is and what is included to make it realistic and why. Solid bench work is a requirement of any layout so that you end up with a clean, smooth operating track. You also must have reliable locomotives and rolling stock. Many aspects of operating a model railway were presented. A system for Train Movement, A system for



PHOTO ABOVE: John Johnston receives Certificate of Appreciation from Tony Czerneda.



PHOTO ABOVE: Golden Horseshoe Members at John Johnston's N Scale Grand Trunk Southern Rwy.

Car Movement, Researching your Model RR prototype, Establishing a database of information about your layout, Identifying Railroads Customers, types of cars needed, Create a car inventory, Train Types by function, by class. Creating trains, Train Lists, Creating train schedules, Traffic Control, Timetables and Train Orders, Track Warrants, Car Forwarding, Wheel Reports, Markers on cars, card car systems and switch lists. John discussed the pros and cons of different car forwarding methods, what an operator needs to know about the model RR and hints for better operation as a host and as an operator. Finally guest operator etiquette was discussed. This was a very extensive clinic.

A break was taken about half way through the presentation for a refreshment break. Tony thanked John for his presentation. Tony said John presented a lot of food for thought and presented a Golden Horseshoe Certificate to John for his excellent presentation.

- Door prizes were then drawn:
- Dundas Train Mug – won by Rob Essery
 - 6060 Mug – won by Jackson McGee
 - Swiss style mini hammer – won by John Johnston
 - Basic Knife set – won by Tony Czerneda
 - Razor Saw and Magnifier Tweezer – won by Bernard Hellen
 - Fine Ballast (Cinders) – won by Tom Allan
 - Fine Ballast (Light Gray) – won by Bill Campbell

Tony thanked everyone for attending and thanked the people from the London and Toronto Chapters. Meeting adjourned at 2:25 pm.

NATIONAL CAPITAL CHAPTER

The National Capital Chapter, thanks to the motivation of

Bruce Leckie, went on a layout tour to Montreal on October 24th. Nine members were able to carpool, from Brockville, Ottawa, and the surrounding areas. Another five or six members expressed interest, but had other commitments.

First up was the new home of the Canada Central Model Railway. You may recall the massive layout under the CN viaduct. Well, they had to vacate those premises, and are setting up in a new location near Highway 40 and the Laurentian AutoRoute, in the basement of an industrial office building. The very high ceilings allowed for massive backdrops and a multilevel layout. As of their open house on the weekend, the basic main lines are laid, and



PHOTO ABOVE: Scratchbuilt suspension bridge rescued from the previous Canada Central layout.

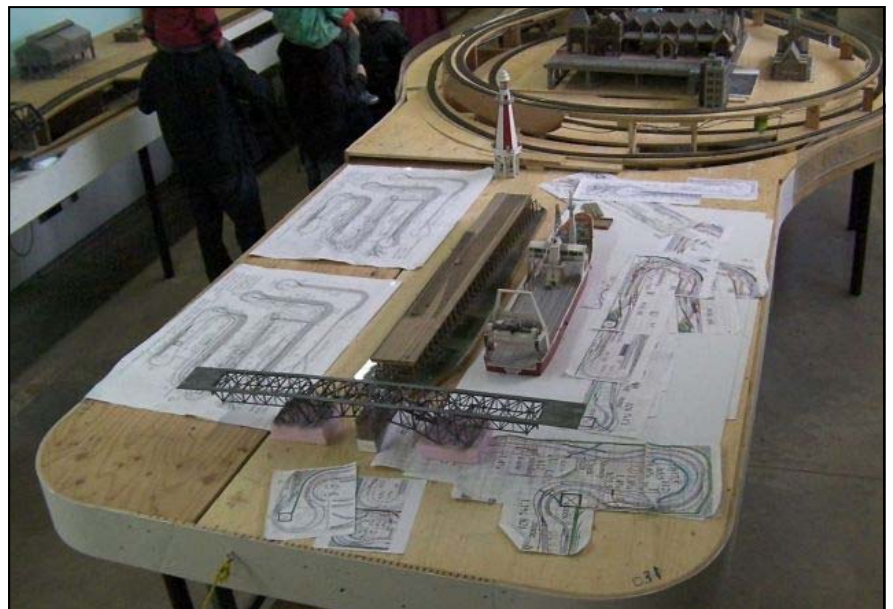


PHOTO ABOVE: Overhead view of the planned port facility showing dock and ferry under construction.

certain corners scenicked. This is an ambitious project, and they are moving forward with energy and dedication. After a group lunch at a local chicken & ribs restaurant, we moved into the West Island Modular Railroad Club, on the second floor of an industrial building in Dorval. This small but interesting layout gladly opened its doors to the CARM NCC members, encouraging them to bring and run trains. DCC equipped loco power was provided for those who did not bring anything. The only interruption was stopping to inspect the many trains passing, right outside the windows. Both main lines of CP and CN, plus the VIA trains, and the local commuter traffic provided the show!

Before heading homeward, all cars stopped at Hobby Junction, a well-known local hobby store specialising in model trains, and train memorabilia. Many thanks to Bruce Leckie for being the prime mover on this event. It would not have happened without his perseverance and communication efforts. Mike Hinds opened the Chapter coffers to subsidize this event, Thanks again, Mike! The consensus of the group was that more similar trips and tours in the future would be welcomed, and participated in. Bruce has already roughed out a list of possible destinations, for stand-alone trips, and also trips involving other model railroading events.



PHOTO ABOVE: At the West Island Modular Club layout, Steve Watson watches Warren Mayhew build a train. Warren is both a club member and a CARM Member.

PHOTO BELOW: Ian MacLeod, Ian Frost, Mike Hind, Steve Watson, Bruce Leckie, John Saunders, Seanna Watson, guest Alex Thum, Rich Stewart



PUBLICATION SCHEDULE FOR *THE CANADIAN*

The Canadian is published four times per year.
Submission by authors or Chapters should be submitted
by the following dates.

Spring Issue: February 1 Summer Issue: May 1
Fall Issue: August 1 Winter Issue: November 1

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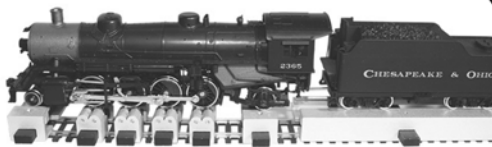
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IAN DIXON'S MYSTERY RAILWAY

TEXT AND PHOTOS BY TED RAFUSE

There is a rationale for Ian Dixon's moniker referring to his N scale layout. It is dubbed the Mystery Railroad of Canada for the simple reason that Ian has not defined its specific locale. It could be in many places in Canada. In effect it mirrors the transition era but not always. The reality is that there are no specifics associated with the layout. Hence it is a mystery railway operating somewhere in Canada.

With the foregoing caveat in mind, Ian has creatively constructed an N scale railway layout in a spare bedroom. But there is nothing spare about the fully landscaped miniature railway. He operates trains daily, or perhaps more accurately every evening, prior to retiring for the night, which he does in another bedroom. The layout was cobbled together of disparate pieces and moved from disparate locales, so before examining the present, an examination of the past is warranted.



ABOVE: The bedroom with bed in the centre and the layout surrounding it on three sides provides a situation where space is at a premium but the modelling is functional. Lacking a wide angle camera lens standing in the doorway barely reveals the three sides of the layout.

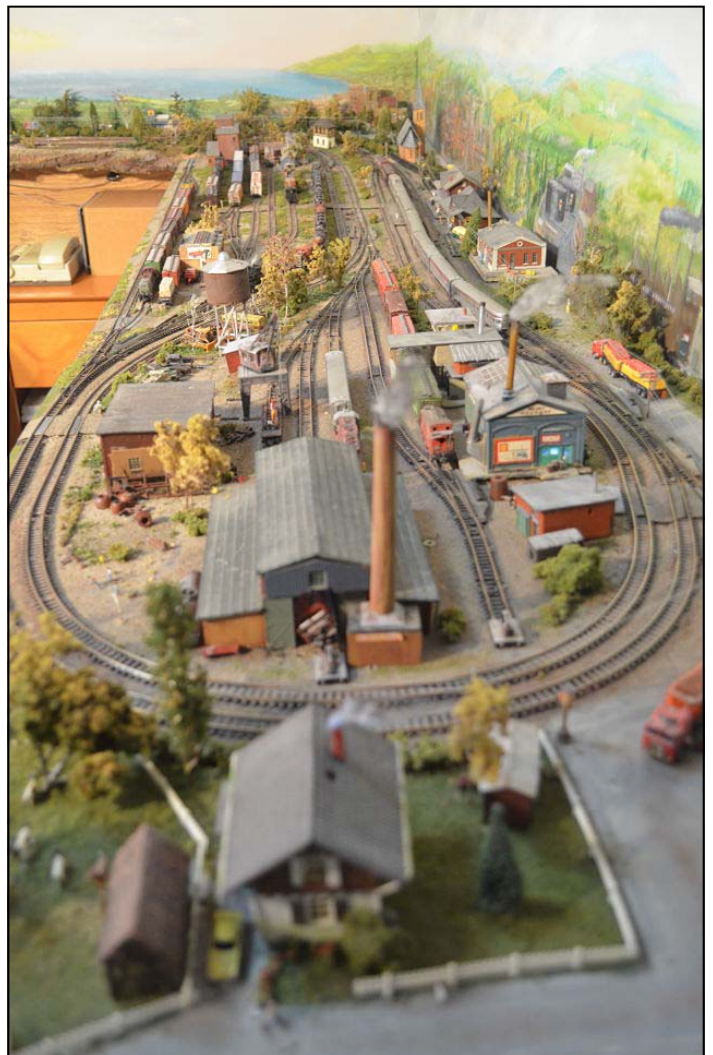
Ian acquired his interest in model trains as a child in Lincoln, England. This first train was a tin clockwork model running on an oval of track. Very quickly frustration occurred with this carpet layout. The frustration was in the oval track that started nowhere and ended nowhere. What he sought was a train that went from somewhere to someplace else and try as he might the wiggly-waggly track plan of circular pieces failed to fulfil the operational concept he sought.

A Triang battery powered OO scale provided an alternative a few years later. With only forward and backward controls operational opportunities were impossible. There

was no speed control in either direction. Fresh batteries hurtled the train around the track at rocket speeds. With new batteries it operated, in his recollection, at a scale speed of 250 miles per hour or so and of course there was no control of speed until the batteries commenced to lose their power. As the batteries ran down, a more realistic speed was obtained. But switching opportunities were still impossible.

Progress came later as a young adult in Scotland when he purchased a OO Hornby set for his son. There the two boys, father and son, created a more operationally model

BELOW: This panoramic view of Huntersville illustrates the mainline loop on the edges of the layout as well as the myriad switching possibilities in the area bounded by the loop. Towards the top is the general freight yard while nearest the camera is the locomotive servicing area which includes an industrial spur with several car spotting locations.





ABOVE: Huntersville, named after one of Ian's grandsons, is situated along the wall nearest the entrance to the layout room. It is the site of much rail traffic and yard activity and industrial switching. People abound on the layout from those waiting on the station platform, to the passengers sitting beside the windows in the passenger train, to persons waiting to board the attending busses, and to workers and pedestrians observed at many locations on this side of the layout.

rail system but this effort was doomed to end as well.

A trans-Atlantic move to Canada in 1978 proved a shock in many ways. Ian brought an N gauge set with him and set up a small layout in the basement of his first home in Hamilton. Not only was the move a culture shock in the broadest sense, it was a shock to the railway sense as well, both full scale and model. It took several years to learn that a guard's wagon was named a van or caboose in his new homeland. Another shock to the system was the lack of available N scale models in English or Scottish prototypes. This prompted a change of scale and with two sons he created a rudimentary HO layout. It did not last long due to a house move and the hobby waned for several years thereafter.

Several moves later landed the family in Peterborough. There he met fellow modeller, Ron Hiscox. In return for painting his backdrops, Ron presented Ian with an N scale layout on a door. This brought Ian back into the hobby and in his favoured scale as well. Other moves occurred and over time the layout evolved into its present U shape that occupies a bedroom. There is sufficient room about the bed for movement from one side of the layout to the other. The layout itself is free standing as Ian does not own the apartment residence and did not want to mar the walls in any way.

No track plan was followed although there is a pretence to its current design patterned from several thoughts that circulated in his head. The original door layout consisted of a figure eight over and under design and the current configuration witnesses this portion as a depressed segment surrounded on the perimeter by a raised track. From this base an original siding, now the mainline, gains access to the other two segments of the layout. There

are three passing tracks and multiple yard tracks which use 23 turnouts. Track and turnouts are from a variety of manufacturers.

A single transformer with several blocks controls movement on the track. An improvement Ian foresees is the installation of double pole double throw centre off toggle switches to allow for two train operation. Motive power includes both steam and first generation diesels from several manufacturers. Most of the steam locomotives have become static models but the RS3s and F7s are the layout's prime movers. Seventy pieces of weathered and graffiti painted rolling stock provide ample opportunity for freight trains running as intercity trains and for switching at several industries and yards along the mainline. A stainless steel CPR passenger train is the pride of the layout. CN diesels appear as the favoured switch engines either singly or in tandem.

Structures are a mix of kits purchased at local trains shows and scratch built using a variety of materials at hand. Ian's artistry appears in these structures all of which are weathered. Many have interior illumination from strategically located LED Christmas string lights. Scores of miniature people provide an additional facet of life on the layout and more than 60 ride in the comfort of the stainless steel passenger cars. And of course Ian's artistry is evident in the background scenes that encompass the layout. Trees consist of enhanced ragweed and former Christmas decorations. With his artistic flare, Ian turns the proverbial pig's ears into silk purses.

Modelling provides both inspiration and therapy for Ian who recently suffered a major illness. The U shape design of the layout forces him to walk around providing therapeutic physical exercise while the modelling and painting provide therapeutic mental distraction.

Ian is a firm advocate of the adage that model railroading is fun. And how many of we modellers actually operate our layouts daily? Ian Dixon operates his mystery railway layout every day. Ian is also a supporting member of CARM, good news for all who read this article.

BELOW: The plastic coaling station has received a subtle weathering application. the steam locomotive appears fired up and ready to move, however, it is a static model but has the appearance of life through the smoke emitting from her stack.



THOUGHTS ON SCRATCHBUILDING

TEXT AND PHOTOS BY GERALD HARPER

INTRODUCTION:

Assuming your layout needs some buildings you have three basic options for how to acquire them:

- 1: Completed buildings from major manufacturers such as Woodland Scenics (www.woodlandscenics.com), Bachmann (bachmanntrains.com) etc
- 2: Constructing kit buildings many of which come with flat wall panels which allow various configurations e.g. Design Preservation Models (www.woodlandscenics.com), Kanamodel "Express", Walthers Cornerstone Series (www.walthers.com) and Hamilton Model Works.
- 3: Scratchbuilding based on plans acquired or drawn and building materials.

The objective of this article is to explain how easy it is to scratch build buildings and what are some of the advantages. The main advantage is that you are not bound by someone else's shape or ideas and you can make the footprint whatever size will conveniently fit into your layout. Let's face it a layout is primarily track on which you can run trains, switch traffic, turn locomotives etc., so the balance of the space that is left over provides a backdrop and a context for your trackage. In terms of context this place has to accommodate cliffs, forests, fields and other elements that relate to revenue generating industries for your layout. Many of the buildings you will need are industrial structures to provide a source of railcar loadings and unloadings.

Walthers, over the last ten years, has manufactured a series of industrial complexes such as steel mills, cement plants, sawmills, pulpmills and agri-silos complexes. Most of these are in components which, after a little kit bashing, can be erected to fit into your space, which is always smaller than you really need and less than Walthers planned for.

I am going to try and describe some of the basics, to guide you in building your own unique structures.

Before we go any further I should note that I will mention various manufacturers and products primarily to give you ideas as to where to acquire items and whenever possible I will add a website link so you can explore further. I will also try and give more than one supplier to indicate that there can be variety and competition in prices. Finally I should note that the "scrapbox" is a vital resource. If you don't have one, start accumulating items into one now. For example keep offcuts of scale lumber because they will be ideal for the junk cluttering the sawmill or construction site you build. A lot of materials are avail-

able free or at very reasonable prices from unconventional railroad modeling sources. Suggestions:

- coffee shop wooden stir sticks (stained with coffee is most realistic), they are perfect 2 x 12s in O scale
- shishkabob sticks for tree trunks or logs in HO scale
- plastic tubing such as drinking straws
- clear plastic packaging from DVDs which is perfect for glazing windows
- left-over sprues from plastic kits for plumbing in your industrial complex

TOOLS AND SCALES:

There are three general types of materials to build with: wood, plastic and metal. Metal is most commonly reserved for locomotives and rolling stock, bridges and elements of industrial complexes. Wood and plastic are to a large extent interchangeable but older buildings are likely to look better when assembled from wood while newer buildings benefit from plastic construction. Note the various tools displayed in some of the pictures that illustrate this article. Almost all can be obtained from craft stores or model railroad hobby stores such as MicroMark (www.micromark.com), Hobbyworx or North West Short Lines (www.nwsl.com) for specialty items such as choppers.

The basic tools you need are common to all building tasks, knives, choppers, saws, squares, rulers etc. For metal you will also need a soldering iron or resistance soldering system and the appropriate cutting and bending tools for metal. I will talk about adhesives later on.

Critically important is a good ruler with the appropriate scale for your railroad. A steel one also serves as a straight edge for cutting, scoring and bending. Almost as important for making life easier is a digital caliper which allows one to measure thicknesses etc., in either metric or Imperial measurements.

Model railroad hobby scales are weird. Almost without exception they were all originally developed using feet and inches as the measurements for prototypes and scale size. However some have since become somewhat metricated and some are even a mix of both dimensional units. They also vary from continent to continent but I am going to stick with the most common dimensions used in North America. The table at the top of the following page sets out the main scales described as ratios followed by simple feet and inches proportions where such are useful.

Scale	Ratio	Feet & inches	Mixed units	Comments
N	1:160		2mm = 1 foot	
HO	1:87			This Half O scale (hence HO) should be 1:96 not 1:87 but somehow evolved to 1:87 in N America. In the U.K. HO is OO and is 7mm to the foot.
S	1:64	3/16 inch = 1 foot		A good scale for availability of vehicles and other decorative items.
O	1:48	¼ inch = 1 foot		Also a good scale for supporting vehicles which are abundantly available at 1:50, but O needs lots of space to build a continuous loop model.
G	1:20.3 and similar scales such as 1:19.1, 1:21		16mm = 1 foot (narrow gauge) and 32 mm = 1 foot	Garden scales vary and are unique to different manufacturers

Many modelers model narrow gauge railroads and to a large extent they have adapted the existing scales to accommodate the narrow gauge appearance. For example On30 which means O scale narrow gauge 30 inch gauge corresponds to HO track with an O scale train running on it. Likewise HOn3 is narrow gauge HO scale trains running on N gauge track representing 3ft gauge *and Nn3 is narrow gauge N scale trains running on Z scale track.

Most building materials are sold with either an Imperial dimension, a Metric dimension or a Gauge. The latter particularly applies to wire and rod which is defined by Standard Wire Gauge. If plastic sheet is 2 mm thick you will find a caliper handy to find out what its thickness is in inches or decimals of a foot so you can multiply it by the scale factor to find the model dimension.

Hereafter my examples will be mostly O scale structures as I have been building them to fit into an On30 narrow gauge modular layout which depicts a British Columbia mining scene in the 1930s. None of the commercial manufacturers make assembled or kit form structures that are appropriate for this vintage or type of structures so I had no choice but to scratchbuild with the help of archival photographs as a guide.

GETTING STARTED

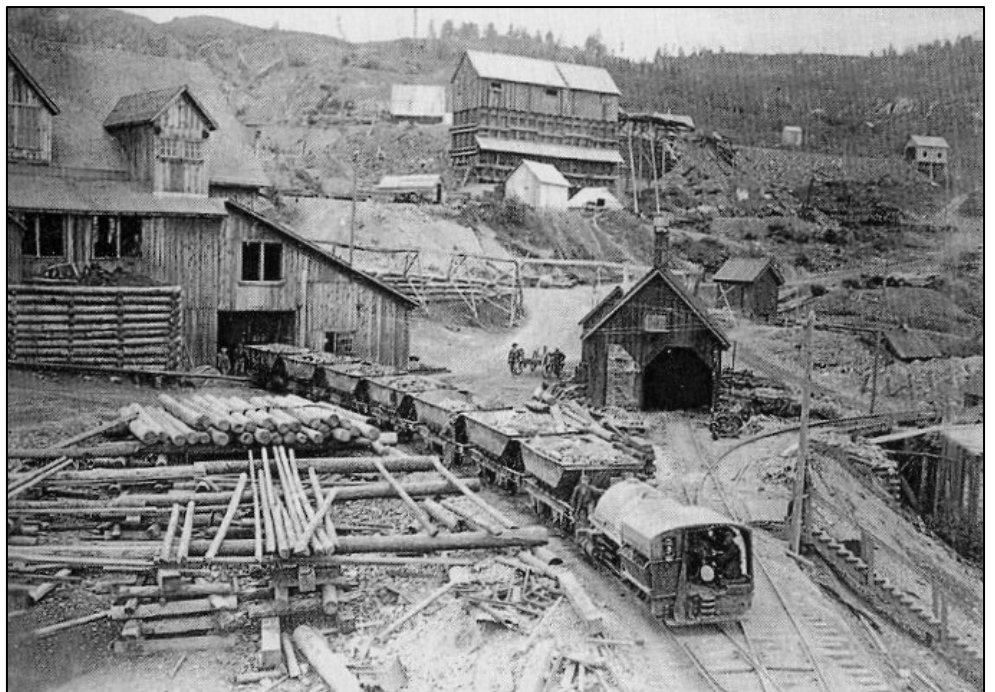
- 1: Decide on what you want to build.
- 2: Determine approximately how much space you have for it and whether that space is sufficient for all the features it might need e.g. a loading shed for a box car must be long enough to accommodate the whole length of the door of the boxcar.
- 3: Research the structure, ideally by acquiring photographs of the

original or similar structures. **PICTURE #1** shows a wooden engine shed as the prototype of the mine I am building.

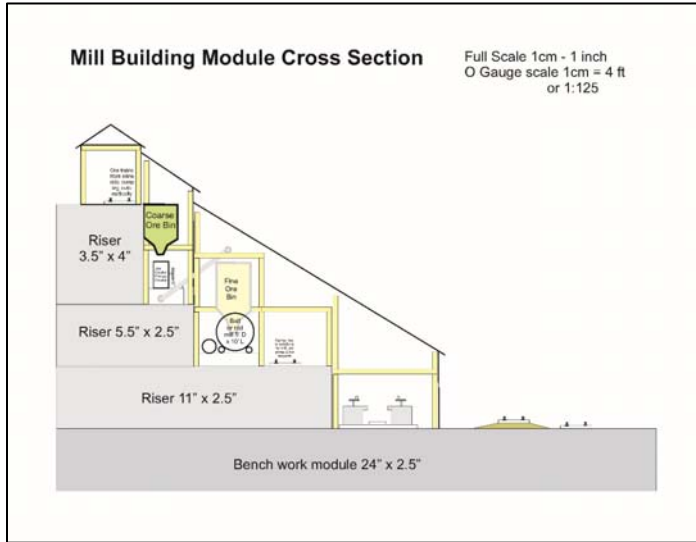
4: Draw some sketches of the floor plan and the elevations, ideally to scale, so you can use them as templates for the building. Several examples are given below. **PICTURES #2,3, and 4.**

5: Think about how to build it, including studying full scale construction methods for similar structures. Don't buy or cut anything until you have slept on it and investigated

PICTURE #1: Engine shed in the Phoenix Mining Complex. The author has built a customised version of this shed and uses it to exemplify many of the techniques described in this clinic. Note the clutter of material covering most of the ground and the lack of any trees until well up on the mountainside away from eager axes. Mines devoured timber so incoming supplies would include a lot of timber.

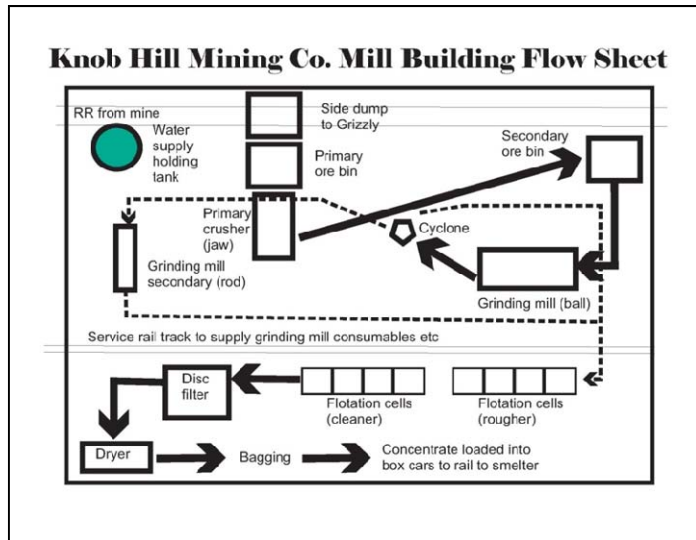


options for materials. If they are not available or very expensive you may want to rethink your plan.



PICTURE #2: An example of a simple graphic to determine general dimensions

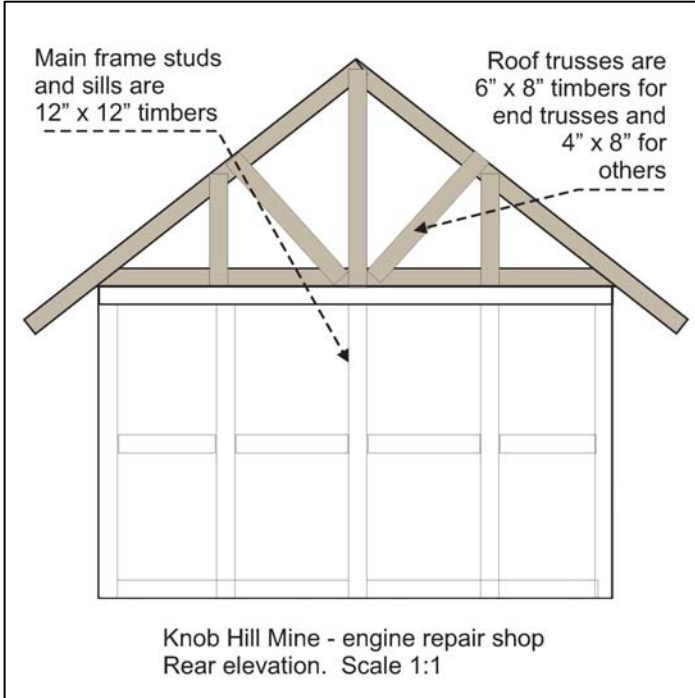
Keep in mind that if you are building buildings without the need for visible "furnished interiors" you can get away with a lot less accuracy in the structural design of the building. The frame simply needs to be adequate to hold the exterior panels at the right angles and it doesn't matter whether it is to scale or not.



PICTURE #3: Layout plan of all the features that need to be inside the proposed building to allow measurement of space needed.

As we progress through this article you will notice that I have turned the engine shed around and brought the smaller shed, just visible at the rear, to the front. But after bringing it to the front I couldn't make it a V sloping roof as otherwise all the snow would have shed onto the railroad access track. So I made it slope away from the track

and heightened the wall. Consequently the steam engines' smoke hangs in the approach where the wall is high, which means that the wooden walls need distinctive staining to reflect the smoke of the steam engines and the exhaust of the diesels. Keep in mind too that my model is set in the 1930s which was a period of economic difficulty and so industries were hard pressed to replace or maintain their items other than by patching and hay-wiring. Note that there is only one shed door. Presumably the other fell off its hinges and not being critical wasn't repaired.



PICTURE #4: Engine shed rear elevation to be used as template for roof trusses and front and rear wall frames

CONSTRUCTION:

With your plans and a shopping list of material needs established you will be able to accumulate the necessary wood or plastic to get started with construction. Most railroad model shops sell several brands of plastic rod, strip and sheet.

Evergreen (www.evergreenscalemodels.com) is the most commonly available brand. For wood try your hobby shop too as it will almost certainly sell packets of scale lumber from Mt Albert Lumber (www.mtalbert.com). Chooch Enterprises sells self adhesive sheets of stone wall which makes good retaining walls or stone foundation layers below brick on walls. (www.ChoochEnterprises.com) If the stores do not have your needs then go on line and try ordering directly. Mt Albert Lumber attends some of the shows in Ontario and also many of the Narrow Gauge shows in the USA and often sells off seconds in bundles which are well worth buying as they can save you a lot of money and the irregularities are usually so minor as to

enhance rather than detract from the look. Remember that scales are relative and a 6" x 6" piece of lumber in O scale is a 12" x 12" in HO or an 8" x 8" in S scale.

Chopping boards as sold by NWSL and MicroMark are very useful for cutting precise lengths and end angle cuts. They work for plastic as well as wood. If you are purchasing one make sure you buy a packet of spare razor blades for it.

If you are building in wood you are going to need to glue members together which requires either white glue (cheapest) or some other modeling glue designed for wood. C.A. glue works well with wood and is exceedingly fast drying but is anathema to some modelers because of its toxicity and ability to attack the skin. As shown in the various illustrations below white glue works very well but

needs clamps, weights, flat surfaces and time to ensure a good solid bond. If you are using plastic then plastic solvent cement is much easier to use but still needs the same clamps and weights and a square to ensure that it dries with right angled corners. With plastic solvent glue I always allow one hour before getting onto next construction step. Never use plastic solvent glue near naked flames even if they are behind a door. Most furnaces have a naked flame so stay away from them with your solvents.

A very useful tip is to have available some kitchen wax paper and tape it down over your layout plan on your flat cutting board and then glue everything on top of the wax paper. Excess glue does not stick to wax paper and it protects your plan underneath so if you need the plan for several walls or roof trusses you can use it without damage.

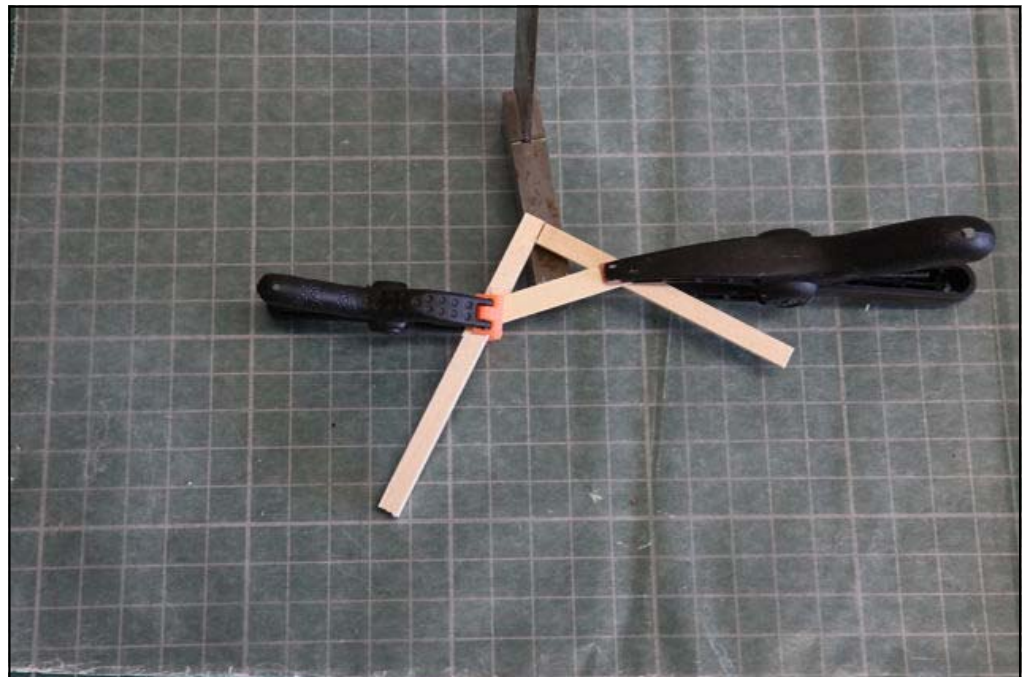


Wipe off excess white glue with a damp rag otherwise if it dries into the adjacent exposed wood surfaces they will not accept stains like the rest of the wood.

Once you have built all the two dimensional components of your structure you need to start the three dimensional assembly. It may be advantageous to use a few clamps and clips to hold it roughly together so as to check that locomotives can get in the doors/openings, curves are not so tight that cars touch the building and other issues. You should also add walls and other elements that reinforce the strength of the framed side as long as they do not hinder you subsequently working on the interior. If you are using

PICTURE #5 ABOVE: Clamps holding a wood framed building side while it dries. Note the background cutting board with rectangular grid lines which helps ensure squareness. The square is also useful to doublecheck all angles. Note the wax paper underneath.

PICTURE #6 RIGHT: Using spring loaded clamps to hold a corner brace until the white glue dries. The trouble with this method is keeping everything square in two dimensions. Note the wax paper underneath.





PICTURE #7 ABOVE: An alternative to the method shown in Picture #5 to glue the corner brace is to keep the frame flat on the cutting board and use a heavy weight to hold the corner bracket in place un til the glue dries. The wax paper is evident.

PICTURE #8 BELOW: Checking the clearance of the engine shed entrance incorporating the height of the track and ballast as well as the tallest smoke stack.

sheet wood scribed like board and batten or any of the various plastic textured sheets that provide a multitude of surfaces remember to cut out the window and door openings while it can be done on a flat cutting board. Three dimensional assembly uses the same techniques as for two dimensional except that you need more squares and more clamps so as to hold everything in more directions. Try and ensure that the surface you build the model up from is flat and level. Before you finally glue and tighten the clamps double-check with your square that everything is correctly angled. Unless you have lots of clamps and blocks don't try to glue all four walls together in one exercise. Rather do two walls at one corner first.

If you are working in wood it is advisable to give white glue two hours to dry between stages so you will only get two gluings done per evening construction session. Plastic solvent glue is much quicker drying so 15 minutes per gluing is probably adequate most of the time. Even I contradict myself when I want to get things built in a hurry.



STAINING AND PAINTING:

You will notice in Picture #8 that the engine shed has variably coloured walls as a result of applications of stain. It is usually advantageous to apply stain or paint to interior walls before the final building assembly is undertaken, interior figures and features added and very definitely before the roof is fixed in place. Wooden buildings usually look best after various applications of various stains. Stains are a dye in an alcohol solvent base so you want to use them in a well ventilated work area. The dye could be something like shoe polish for black. They can be made up yourself but Hunterline (www.hunterline.com) sells a tremendous range of colours including railroad creosote black, rust, cordovan brown etc. Hunterline attend many shows around the continent each year and by checking their website you can find the list of shows they will be attending as well as a colour chart of their range of stains. I use their products and if you will see in Picture #8 that I used barnboard appearance on the outside walls and a warmer pale brown for interior walls. I then highlighted the exterior wall above the engine entrance peak with black stain applied very weakly so as to emphasise the increased sootiness of the wall immediately above the smoke stack. I also applied a slightly darker more grey toned stain to the roof rafters as they would have accumulated a lot of soot from constant locomotive traffic and such areas wouldn't have been cleaned. If you are not sure what effect you want then apply some stains to scrap offcut pieces of wood and let it dry before deciding.

If you are building in plastic then stains cannot be applied and paint or stick on coloured sheets are the solution. Acrylic paints work well and are available at low cost in a wide range of colours from craft stores. Acrylic paints are also water based so the brush and painted area clean up is easily done with a damp rag. Patterned sheet can be purchased or found on the web and then printed out. This glues easily to styrene sheet.

Window frames should also be pre-painted whenever possible so as to avoid the risk of an unsteady hand streaking the wall with contrasting colour paint. That is why Picture #8 only shows one window frame installed.

WALLS AND ROOFING:

Before you start to clad your framed building you must determine what you want to put inside it in the way of machinery, furniture, people, staircases, internal walls as well as lighting. It is easiest to install these items while the walls and roof are open other than studs and rafters. Picture #9 shows a mill building that I constructed which has a wealth of interior detail which could never have been installed without unhindered access through the roof. I have no intention of building a permanent roof but maybe just a lift-off version so people can see the detailed interior.

PICTURE #9: Mill building with complete set of interconnected machinery, piping wiring and access stairs, walkways and people.



Older structures are likely to have wooden clad walls and several wooden and plastic sheets are available cut to represent shiplap, batten and board etc. Mt Albert Lumber, that we have referenced before, sells such sheets in wood and Evergreen sells styrene versions. Newer industrial buildings also usually have sheet coverings of some sort whereas newer residential walls may well be brick or stone. Plastic sheet representing those are available. Paper printed sheet brickwork is also available which can be glued onto smooth sheeted wall covers with the window and doors holes previously cut out. For my mill building in Picture #9 I used coffee shop stir sticks including the warped ones so that I had cracks in the walls which added to the effect and allowed me to have some of them patched over.

Roofing and also walls on many industrial buildings were clad with corrugated iron. Several companies make corrugated iron by either rolling sheet aluminum or thin cardstock through rollers with the appropriate sized grooves for the relevant scale. I personally prefer the sheets made out of aluminum rather than cardstock because it looks shinier like new galvanized sheet iron and secondly it takes a light airbrushing of rust coloured paint better. Many older buildings have rusty corrugated iron, sometimes interspersed with a few new sheets where leaks forced the owners to spend money on fixing it. Stoney Creek Designs makes a range of roofing supplies including corrugated aluminum sheets in correct scale sizes as also do Sodders Enterprises (<http://tes.wirefire.com>).

Tar paper is another common roofing material, particularly for raiiside sheds. Several articles have described how to make tar paper look realistic by cutting strips of tissue paper to the appropriate width, soaking it in diluted white glue and laying it on a sheet roof. After it has dried it is painted the appropriate dull black and tears, cracks and other imperfections incorporated. Flat concrete roofs can be made in a similar way and fine sand should be mixed in with the paint to give a gritty texture.

Roofs of tile require purchase of the appropriate plastic moulded sheet. Again Evergreen makes many such pat-

terns and also does an English company called Wills (www.peco-uk.com). North American shows attended by dealers offering UK railway items will almost certainly have a box full of Wills products.

THE FINISHING TOUCHES:

Your building is not finished when it looks complete because it will sit on your layout looking lonely. It needs to be blended into its surroundings. Whether that means creating a paved parking lot round it, a busy street running by it, a ballasted rail yard right up to the walls or weeds and junk, any and all may be appropriate to provide the completed setting. Residential structures sit in surroundings which mostly haven't changed much in 150 years, except for the vehicle in the driveway. Commercial structures likewise face onto sidewalks and streets or the backs of them provide junk yards and additional storage areas. Viewed from a railroad you are most likely to see the back of a building. Industrial structures are always surrounded by the components of their trade, whether it is stocks of finished product, supplies of raw materials or relics of earlier versions of the machines in use therein. You can let your imagination run wild, the contents of your scrap box get utilised and lots of rust coloured paint or powders applied to everything metal.

If you have totally run out of ideas and junk then you can go on line and find companies that sell junk. Berkshire Valley Inc. sells castings of large and small junk piles. Schomberg Scale Models sells barrels, loaded sacks and other individual items to populate your yard.

Pictures #10 and #11 show examples of placing the building into its environment.

PICTURE #10: Customising your building by covering over window panes with plywood where the maintenance man had run out of glass to replace broken panes and adding action figures doing meaningful tasks. Sacks of copper concentrate are being readied for loading into the train when it arrives.



PICTURE #11: Blending the building into the background is important. Weeds and junk help the transition. The door with the broken hinge suggests it isn't used much but the well worn path through the weeds to the junk pile suggests that the junk gets picked over regularly.

AFTER THOUGHTS

After you have completed a few structures you will probably start to develop some pride in the best ones and that pride is usually very well deserved. So build your buildings so that they are removeable, put them on a temporary background sheet of plywood and take them to your next train show to win a prize or have them photographed to appear in *The Canadian*. A recent issue highlighted several of the excellent models displayed at the CARM 2013 Convention in Thunder Bay. The Copetown Show displays models and so does the Schomberg Show. See you at one of those shows. I will be the guy with used coffee stir sticks and a scale ruler in my shirt pocket, taking photographs of everything.



TRAIN SHOWS

Sunday, January 10th, 2016, Woodstock Model Train Show And Sale. Featuring vendors selling Model Trains and Railroad Memorabilia; plus several Operating Layouts. Oxford Auditorium and the Mutual/Market Building, on the Woodstock Fairgrounds, 875 Nellis Street. Hours: 10 a.m. to 3 p.m. Admission: \$5.00 per person. For more information call Ian at 519-426-8875 or email: toyshow@kwic.com Show Website: www.woodstocktrainshow.blogspot.com

Sunday, January 17th, 2016, 2016 Paris Junction Model Train Show Sponsored by The Western Ontario Division - NFR - NMRA. Featuring Operating Layouts, Vendors and NMRA Craftsmen tables. Paris Fairgrounds 139 Silver Street, Paris, Ontario. Hours 10 a.m. to 3 p.m. Admission: General \$4.00; NMRA WOD Members \$3.00; Children under age 12 Free For more information contact Edward Howes, Train Show Coordinator at 519-442-7193 or email: ehowes@boardresources.com

Sunday, January 31st, 2016, T,H&B Model R.R.. Flea Market. Presented by the Rail-Ops Club. Marritt Hall Ancaster Fairgrounds, 630 Trinity Road, Ancaster. Hours: 10:00 a.m. to 3:30 p.m. Admission Adults \$5.00. Children under age 12 free. For more information call 905-335-9112

Saturday, February 20th and Sunday, February 21st, 2016. Barrie Allandale Model Train Show. The largest train show north of Toronto. Featuring vendors and many layouts. Bradford Greenhouse, Highway 90 and Highway 27, Barrie, Ontario. Hours: Saturday 10 a.m. to 5 p.m.; Sunday 10 a.m. to 4 p.m. Admission: Adults \$7.00; Seniors and Students \$5.00; Children under age of 6 are admitted for free. For more information contact Paul Martel at 705-431-4330 or

email: the.martels031@hotmail.com

Saturday, February 27th and Sunday, February 28th, 2016, Essex Model Train Show, Essex Public School 72 Brien Street East, Essex. Hours: 9:30 a.m. to 3:30 p.m. (both days). Admission: Adults \$5.00 (each day); Seniors and Teens \$4.00 (each day), Children \$2.00 (each day); Ages 3 and under Free. For more information call 519-776-9800 or email: heritageessex@bellnet.ca

March 5, 2016. Cobourg Model Train Show presented by the Cobourg Model Railroaders. Lions Community Centre, Elgin Street East, Cobourg, Ontario. Saturday only 10am to 4:30pm. Adults \$5, Children under 12 \$1. Info: Phil Niles 557 Willow Crescent, Cobourg. K9A 2B2, 905-372-0481 or philip.niles@sympatico.ca

Sunday, March 20th, 2016. Kitchener Model Train Show And Sale. Featuring vendors selling Model Trains and Railroad Memorabilia; plus several Operating Layouts. Bingemans Conference Centre (Marshall Hall), 425 Bingemans Centre Drive. Hours: 10 a.m. to 3 p.m. Admission: \$5.00 per person. For more information call Ian at 519-426-8875 or email: toyshow@kwic.com Show Website: www.kitchenertrainshow.blogspot.com

Saturday, March 26th, 2016. Brantford Model Train Show. Featuring vendors selling Model Trains and Railroad Memorabilia; plus an operating Layout, Best Western Brant Park Inn, 19 Holiday Drive. Please note the special Saturday date for this show. Hours: 10:00 a.m. to 3:00 p.m. Admission \$5.00. For more information call Ian at 519-426-8875 or email toyshow@kwic.com

Gerald Harper's Photos of his Homes Club Layout Tour Visits



PHOTO ABOVE: Craig Webbs On3 Algoma River Railway
PHOTO BELOW: Ross Oddi's freelanced HO layout

