

# PROMOTING CARM

*“How Can I Help” see page 6*



a quarterly publication of the “Canadian Association of Railway Modellers”



## THE CANADIAN ASSOCIATION OF RAILWAY MODELLERS

Founded October 15, 2003  
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**FRONT COVER PHOTO BY RANDY SCHNARR:** An overview look at the N Scale diorama "Arrivals and Departures The CPR in Grey County" which is located at the Grey Roots Museum and Archives in Owen Sound, Ontario.

# CARM SUPER MEET

## “Opening Doors” Layout Tour Montreal August 5&6, 2017

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This event introduces a new format for CARM Conventions, Mini-Conventions, or Meets. We have seen the success of other events such as the Narrow Gauge Convention, and will be adopting that format. Each attendee is responsible for their own accommodation and transportation. We will provide assistance by identifying local hotels and their price options.

The Meet dates are: 5 & 6 August 2017. These dates were selected to take advantage of the long weekend in Ontario

We are lining up a number of events including a Meet & Greet, a Railfan guide for Montreal, a visit to Expo-Rail, private layouts to visit on the way to and from Montreal, and 7 club layouts in Montreal itself.

Each attendee will received a Tour Booklet and Pass, however, Montreal is a large city, and a GPS would be a real asset in getting around.

The 7 club layouts in the Montreal area are: West Island Modular Railway Club, British Model Railway club of Montreal, Vermont & Essex, Chemin de fer Richelieu & St Laurent Railway, Montreal Live Steamers, Green Valley Short Line Railroad Club, and JAC Hobby (2 scales). In addition there are many model displays at the Exporail Railway Museum in addition to their prototype collection.

Among the Hobby Shops available for you to visit are: Hobby Junction, Udisco, Van Horne Hobbies, and JAC Hobbies. We are looking into Hotels which are close to the Dorval Train Station, the Airport and Ian MacCleod’s home club the West Island Modular Railroad Club.

**WATCH THE NEXT ISSUE FOR MORE INFORMATION**

**CPR’s first diesel engine #7000. Built in 1937 by National Steel Car in Hamilton.  
This 550 hp switcher can be seen by visitors to ExpoRail.**





## ABOUT MANY THINGS

This is another of those columns where I am going to muse on a number of topics.

### MONTREAL MEET

Last year we attempted to put together a Meet in the Montreal area. Unfortunately, we underestimated some of the lead time required to organize things and we had to postpone it. This year we have gotten out ahead of the curve and it looks like a nice program involving Delson and a number of Layout Tours is being put together.

One of the issues that has arisen at past Conventions and Meets is that as our numbers have gotten smaller the financial risk in utilizing the traditional convention model has increased. We have looked at how the Narrow Gauge community has organized their Conventions in which items such as accommodations, meals, and transportation are left in the hands of each individual and we are going to give that model a try. Montreal will give us a good sense of whether or not this can be successful. We look forward to your support and participation.

### OPEN HOUSES

I enjoy Open House Tours which is why I have opened my layout for at least a dozen of them, if not more, over the last number of years. They give me a chance to share what I am doing, to enjoy some very constructive dialogue with my peers, and overall, they enhance my enjoyment of the hobby.

That being said, I want to share a story with you from this year's Homes Club Tour. I had about 60 people go through the layout. The morning was quiet with only 3 or 4 people but in the afternoon we got quite busy and there was often 8 to 10 people in the layout room. This made it difficult to keep your eyes on everything and still run trains, talk to people, and be a good host.

As I was answering some questions, I briefly noted a couple of individuals looking under the layout at the wiring including reaching up and pointing things out. At the time, I didn't give it much thought. Four or five minutes passed and then I got that noise you don't want to hear at an open house, the beeping of a dead short somewhere on the layout. It quickly became clear that it was coming from the Tortoise machines and not the Digitrax Command Station as trains were still running. Since the room was full and I had the Loop set up, so there was no need to throw switches, I cut off power to the Tortoises, the noise disappeared, and I was able to turn

back to my guests.

After 15 or 20 minutes, the visitors had departed and I had a few minutes before anyone else arrived so I turned my attention to trying to solve the problem. Turning the power back on, I tried to isolate the sound, but it was coming from all of the Tortoises, so I knew that there was a dead short in the entire system and not just one machine. I checked the transformer leads to the bus and that looked fine. I followed the bus to each of the terminal strips and everything there looked fine. I shut the power off again, and began to go over in my head the possibilities with nothing coming quickly to mind. Then it was like a lightbulb went on. I remembered those two guys looking under the layout with their hands.

They had been on the other side of the room from where I was located but I did remember where I saw them and I went over and looked underneath the layout. I had turned the power back on and the short was still there. I started to move the same wires they had been looking at and all of a sudden the short disappeared. I touched the wires again and the short came back on. Getting my head underneath the benchwork the problem became quickly apparent. In moving the wires they had dislodged two wires from the DPDT switch that controlled the Tortoise. I use Quick Disconnects, and unfortunately, this switch had two that were a loose fit and so when they had moved the wires the Disconnects had separated from the DPDT switch and were now touching one another creating a dead short. At that point it was a quick and easy problem to solve.

It does however remind me of a column I wrote last year about a Layout Tour involving Craig Webb when a book he had on a desk went missing during an Open House. When we are on a Layout Tour, we are guests in someone's home. We need to treat their home, and their Layout the way we would expect our own to be treated. Namely, Look, Don't Touch. If you are interested in something ask the host and if you want to touch something, ask permission. Even though what you are doing may seem innocuous, it may result in unintended consequences.

Has this put me off Open Houses, Absolutely NOT. It falls into the minor inconvenience, lesson learned, category. If there is a layout tour in your area, participate, open up your layout, share your work with others, but give consideration to some signage, saying Look, But Please Don't Touch.

### LAYOUT DESIGN

CARM Member Ken Layland is also a member of the round robin group that I belong to. Several years ago Ken decided it was time to build a layout and started out on a design. It is a small space, about 12X12 and Ken planned out a two level around the room layout with a centre peninsula. It would also have a helix in a separate room.

Both myself and another Club member, Justin Parry shared with Ken our thoughts on layout design and what we had learned from the layouts we had built. 4 for me and half a dozen for Justin. The most important lesson being leave space for people. It took a little bit of doing but Ken listened and the centre peninsula came out of the plan. This meant that the space went from having an L shaped 24 inch wide aisleway to having a centre pit that was about 8X8. As originally designed the layout also had a duckunder to get into the room. Justin and I had both previously designed layouts with duckunders and had subsequently redesigned them to take the duckunders out. We began work on a two level gate, and thanks to another club member, John Henwood, we now have a smoothly functioning two level gate to get into Ken's layout room.

So, why I am I telling you this. Well, one of the nice touches that the HOMES Club has for its tour hosts is that the week after, they open a couple of the layouts and the tour hosts get a chance to visit them. It is a couple of different layouts each year. This year one of the layouts we visited was that of a local MMR. The layout was near completion and was beautifully done and full of craftsmen quality structures. Interestingly, it was in roughly the same space as Ken's, two level, with a centre peninsula, a duckunder, and 24 inch wide L shaped aisle. Sound familiar. To get inside the layout we had to get down on our hands and knees and crawl in. Once inside, two adults could not pass another in the aisles.

As we left, Ken took me aside, and simply said thanks. For what I asked. For talking me out of the centre peninsula and for adding the gate, he said. He still had lingering thoughts about his original design and what he had given up to create people space and make the layout user friendly. Not any more. This particular layout tour had been a unique opportunity to see a benchwork footprint that mirrored his original design and to appreciate what it offered versus what he finally built and to gain the insight that a layout is much more enjoyable when it leaves space for the people who are building and operating it.

## LOCOMOTIVE SOUND

Over the last year, I have been exposed to sound as part of the modelling environment. I am finding myself really conflicted. On a positive note, I like the dimension that it adds. I operated on Ron Tuff's BCR layout, and Justin Parry (the engineer) and I (the conductor) took out a way freight with an SD40-2, with sound (that Ron had muted to a low level), and it was really enjoyable. I also recently received delivery of a PRR M1 Mountain with sound, that is a truly beautiful model.

On a negative note, as I listen to the sound for an extended period of time, it starts to grate, and I find myself turning it off. Muting it such as Ron did with his SD40-2 seems to be part of the answer, however, that raises its own set of problems. It turns out that re-programming a sound decoder is not straight forward. For starters, you need a booster which is set in line

with your programming track. Justin purchase one, but it turns out that it programs Athearn and Intermountain, but not Atlas locomotives. The Soundtraxx booster only reprogrammes Soundtraxx decoders, and on it goes. Maybe its my age, but I'm getting turned off by the complexity which is being added to running trains. I still feel like for me the jury is still out on the whole sound issue.

## TRACK CLEANING


In the October 2016 Issue of Model Railroad Hobbyist which is available online at [www.model-railroad-hobbyist.com](http://www.model-railroad-hobbyist.com) there is an interesting article on cleaning track. The author Mark Gilger uses Woodland Scenics Dust Monkeys which attach to the axels of a freight car and an electrical contact cleaner DeoxIt5. It is a really interesting article and approach which is worth checking out.

**JOHN JOHNSTON: EDITOR**




**PROMOTING THE HOBBY OF RAILWAY  
MODELLING IN CANADA**

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## CHAIRMAN'S REPORT

The problem with winter is there are not enough hours in the day to do all the modeling things one wants to do. We never deem a model railroad to be finished and complete. Every time we go to a store or a show we see something that appeals and which one would like to see on one's layout. If it is a freight car it is an easy addition but if it a building or backdrop feature it probably needs a re-work of the scene to provide an access road or a railroad spur to service the new industry. And that is why this hobby is such fun because it is never finished and therefore never provides the opportunity to be bored.

However with Christmas coming there are a few other things that become priorities to squeeze in. If you live near a Canadian Pacific Rail line check their website to determine if their Christmas train stops so you can visit it. I go every year, usually with one or more grandchild, and almost always forget to take a food donation for the local foodbank which CP collects for. This year I have made multiple notes to ensure I do take such a donation which is for such a worthy cause. For those who do not live near CP you may be able to participate in the Ontario Northland Christmas trains and Santa events which are organised entirely by ONR volunteers. Finally if you are lucky you may spot one of GO Transit's MP40 locomo-

tives that has been decalced in a Christmas livery. When it went past my office this afternoon it was couple to a complete train of new paint scheme coaches.

Another activity I try and organise during the holiday season is an operating session on my layout for grandchildren and their friends, all of whom are amazed and have a wonderful time. It is amazing how quickly an eight year old picks up the features of a DCC throttle. One of those enthusiastic children could become a modeller in years to come.

If you send Christmas cards then think of sending one with a railroad related scene as the picture. At the last two train shows, that I have attended, there been vendors selling seasonal cards. Sending such cards supports a local business person and ensures that your card will be unique among the collection received by your friends and relatives. Christmas Greetings to every one of you and I hope you have an enjoyable holiday season and are rewarded with those things you wish for and maybe wrote an e-mail to remind family about.

**GERALD HARPER: CARM CHAIR**

## PROMOTING CARM: WHAT CAN I DO TO HELP?

Growing the membership of CARM is the most important objective for the Association. No organization stands still and if it is not growing then it is declining. Greater membership strengthens the Chapters, provides opportunities for new Chapters, increases participation in the on line chat room, increases the number of members keen to contribute photos to the calendar and articles for the Canadian and overall brings each member more benefits. At the most recent Executive Meeting there was a lively discussion on how to promote CARM. As you know we have eliminated Membership dues for a period of time in an effort to attract new members. For a variety of reasons, including illness amongst Executive Members, and Internet membership issues, little has been done to date. It is time that we changed that and tackled promotion more aggressively.

We now have a class of Membership which is free and will remain free for the future. The Internet based, non Calendar membership. It is this Membership which we should be aggressively marketing. There are two principal ways we are discussing, Trains Shows, and Clubs. First, let us talk about Train Shows. We require volunteers who will attend Train Shows on our behalf and pro-

mote CARM. Many of you attend some Train Shows already so this could be a win win for you. To assist in this we will pay the cost of a table where it is required. Additionally, Steve Hoshel is developing Banners and Promotional Material which we anticipate will be available directly from him or the local Chapter.

Second, we need Members who belong to Clubs or who have Clubs in their vicinity to talk to Club Members and encourage them to join CARM by taking out the Internet Free Membership. All we require is some basic information, including, name, phone number, and an email address. As many of you can attest to, email addresses often are changed for a variety of reasons and a phone number provides an alternative method of communications.

There are going to be roles for Chapters and individual Members to play in this endeavour. It will take several months for the material to be developed, however, if you are interested in helping, please let Gerald Harper, or Steve Hoshel, know and we will be in touch once everything is set. Check page 2 for their contact info.

## ONTARIO MIDWESTERN CHAPTER

The Annual Fall meeting was held at the Grey Roots Museum in Owen Sound on October 16th. In attendance were Graydon Hancock, Steve Hoshel, Paul Korhonen, Mike Pickup, Dolf Roelofsen, and Randy Schnarr.

Steve opened the meeting with a welcome to all. The Agenda, Prior Meeting Minutes, Financial Statement, and Show Report were all approved. Paul presented the group with a cheque for \$167 for their support at the Grey Central Show. All members marveled at the detail of the N Scale model of Owen Sound in the Grey Roots Museum. We were pleased to meet some of the members of the group of five that built the display.

Promoting the Hobby: Model RR Venues to attend.

October 29: Boomer Auction, Harriston

November 5: Hamilton Layout Tour

November 6: Kitchener Train Show, Bingemen Park

February 18,19: Barrie Train Show

February: Bruce County Museum Heritage Days

March: Copetown Model Train Show

Dolf brought a model train diorama in a peanut butter jar. A good story in a small space. Steve brought drawings of his current project to build an HO Scale road grader.

Peter Stamford our long time friend and CARM member is experiencing a chronic debilitating illness. He is now living at Brucelea Haven in Walkerton. Peter gifted his

layout to a friend and it is now moved. Some rolling stock will be gifted to the Bruce County Museum in his name. Peter, our hearts are with you in these troubled times.

The Bruce County Museum Railway project received a cheque for \$15,000 to kick off the construction of a 5ft by 26ft, 3 level series of dioramas telling the story of the railway in Bruce County. Photos will describe all other towns on the CNR and CPR rail lines. Completion date is June 2018.

Paul moved the Chapter subsidize 50% of the purchase of Chapter golf shirts or long sleeve shirts for each member. The meeting adjourned at 2:00 pm. The next meeting was set for April 9th, 2017 at the home of Paul Korhonen in Meaford.



**Photo Right: Dolf's diorama in a jar.**

**Photo Below: Group looks over the Owen Sound diorama.**



## LONDON AND AREA CHAPTER

On September 10th, the London and Area Chapter held a presentation day in Centralia, Ontario. Peter Mumby was the first guest presenter, and his slideshow of a variety of railway scenes from across southwestern Ontario proved to be very interesting for the group. Included were slides from the local area, London, St. Thomas and the CASO subdivision. Peter has spent countless hours collecting and researching his slides, and it was certainly interesting to see so many different points of view. Following Peter's presentation, Don McQueen spoke on his newest book, "Canadian National Steam!". For those who do not believe that locomotives the likes of camelbacks and compounds were ever used on the CNR, Don will prove you wrong! There are many reference points and Don has done a fantastic job with the research into each different type of locomotive, and anyone looking to model a rare type of CNR locomotive must consult Don and his books.

After a delicious lunch of roast beef and sandwiches, the group had some time to catch up with old friends as well as making some new ones. The second portion of presentations began with William Waithe on his N scale CNR Weston sub. William had recently torn down his old layout and moved into a high-rise condo, and has started rebuilding his layout, which has required a lot of ingenuity and unique ideas! William has shown that as long as you have the passion for modelling, you can build your layout anywhere! We hope to continue to see his efforts and a finished layout appearing in this newsletter in the near future!

Mike Walton was the next speaker up, and his presentation centred around the prototypical operations of his OO Lostock Junction Railway, and the art of train control,



**PHOTO ABOVE: From left to right; Dick Walker, Peter Mumby, Al Drais, Rob Essery, Robert Langlois, and Jason Essery.**

schedules and signalling. Mike's layout has operated for 14 years and has had 150 operations without a systems failure, due entirely to the engineering Mike has put into his operating system. A detailed presentation allowed anyone to understand the intricacies of operating through train order and timetable operations, which run on a 19 hour timetable sped up to cover a two and a half hour operating session. A very good system, and one which many modellers who wish to be as realistic as possible should aim to achieve.



Last but not least on the presentation circuit was a demonstration by Robert Langlois on the benefits of the S-cab system, which replaces the conventional track powered locomotives with battery power packs instead. Robert has experimented with several British O scale locomotives, and demonstrated the benefits of what battery power can do. The group then thanked everyone who spoke at the day and went to visit two local layouts. Rob and Jason Essery's HO CNR layout and Bob Robilliards' N scale layout. It was a great day to enjoy with everyone who came out, and special thanks go to the presenters, Robert Langlois for the gift plates, and Broughdale Hobbies as well as everyone else who donated items for the raffle. Thanks go out also to Bruce Harmer and Rob Essery for their hard work in planning for the day, and to the Centralia United Church for providing the lunch.

**PHOTO LEFT: From left to right: William Waithe, Mike Walton, Don Wesley, Bob Robillard and Paul Ross.**



## NATIONAL CAPITOL CHAPTER:

At The end of October, the National Capital Chapter went to visit the Belleville Model Railroad club, where they hosted an operating session for us. This club models the approximate environs of Belleville, with a focus on operation, and runs from Montreal Staging through Cornwall, Brockville, Kingston, Napanee, and Belleville to Toronto staging. Most of the visitors were paired with a club member as conductor and dispatched by Rick Potter from a separate dispatching office.

The club occupies an area shaped like an L that is 27 feet on the long sides and 13 feet on the short side. It is constructed using a sectional method of benchwork and has 160 feet of mainline and uses digital command control.

There was a good selection of trains, ranging from passenger, to wayfreights and no major mishaps ( a testament to good planning) After the morning session, we retired to a nearby restaurant for lunch and then the entire group ( CARM and Belleville club members) took a short trip to visit the Brighton model railroad club. This is a modular club that has a semi-permanent setup in a church hall in Brighton. The club has not embarked on an operation scheme yet, but they have lots of industries,

nice scenery and run point to loop.

Our next event is planned as a Christmas social and is tentatively scheduled as a pot-luck lunch at a member's house. In the new year, the chapter plans a major excursion to attend the Double Headers layout tour March 25 in the KW, Cambridge, Guelph area, and we extend an invitation to all other Chapters to join us.

All photos by Bruce Leckie.

**Photo Below: Belleville Model Railroad Club Treasurer Doug Green explains the operating procedure to Garry Comber while Paul Anderson listens in.**

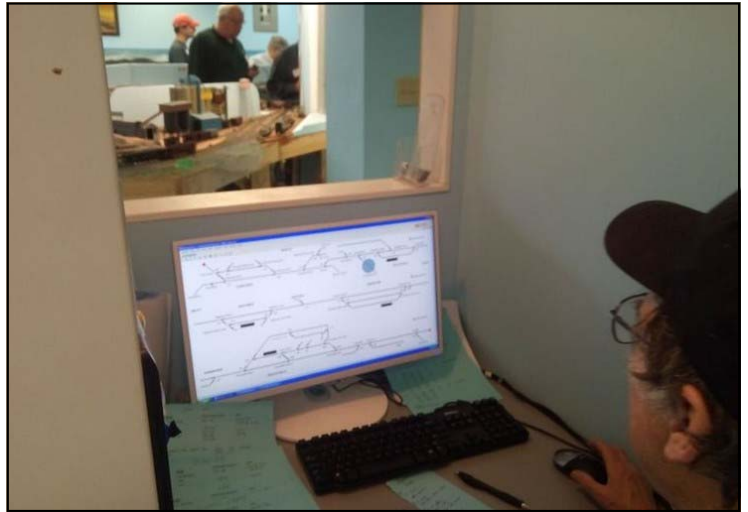


**Photo Right:** Rich Stewart and Alex Thum check the action on the Brighton Club's layout.



**Photo Below:** Ian Frost, guest Alex Thum and Peter Jackson admire the town of Kingston on the Belleville Club's layout.





**Photo Above Left: Rick Potter, Secretary of the Belleville Club is working dispatch this morning.**

**Photo Above Right: The dispatchers view. This screen controls all of the mainline turnouts.**

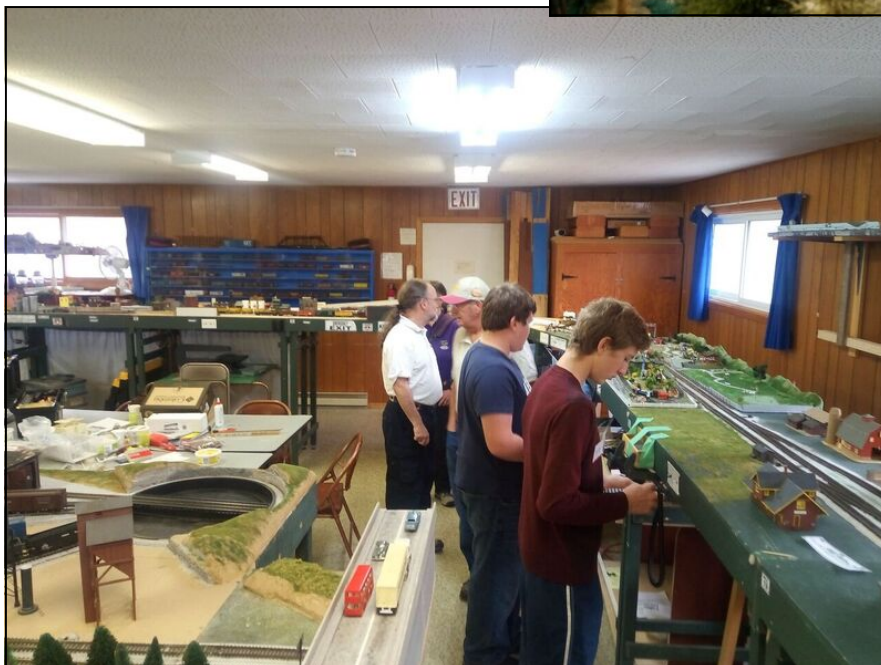
**Photo Below: Rich Stewart and Alex Thum check the action at the Brighton Club Layout.**





**Photo Left: Paul Anderson is busy talking trains with Bob Coker, Brighton Club President.**

**Photo Right: Broad curves on the Brighton layout make for an impressive run as Al Heard, Treasurer of the Club controls a way freight thundering by.**



**Photo Left: Steve Watson watches while Joseph and Ethan of the Belleville Club are busy running trains on the Brighton layout.**

## **Bob Duncan's, HO Scale Northumberland Hastings & Haliburton Railway Text & Images by Ted Rafuse**

Those readers with a keen mind will recall Bob Duncan's N scale Prince Edward County Railway appeared in an earlier edition of *The Canadian*. But, and there is always a but, there is a new railway in his basement. This incarnation is the Northumberland Hastings and Haliburton Railway of Central Ontario. The name reflects the region that Bob has lived in all of his life and he wanted to reflect that reality in his new basement.

When Bob moved it was time to rethink his modelling. With much trepidation he opted to change scales to HO. The new basement afforded a larger space initially and a layout was established. Management then determined

that the space would be better used as a bedroom and hence a new locale within the basement became the site for this layout. The layout is oval shaped with a leg off one corner of the oval for a major town. The oval itself is approximately 16 by 9 feet. To gain access to the centre of the oval there is a hinged bridge that rises upward. It is a piece of wood with track on it and cardboard sides to prevent catastrophic accidents while in operation.

Blue Styrofoam provides the landscape base and hills are formed of the same material, both painted appropriate colours in accordance with their particular feature. The track plan is simple, a twice around the oval with the

Harper's Landing connects the NH&HRy to the outside world through a connection with the CNR. The town serves a number of enterprises illustrated by flats on the near far wall while farther along are various commercial structures in the heart of the town. The small yard marshals both in bound and outbound cars: the county is known best for its agricultural products which are sent west to Toronto and east to Montreal. The station appears in the upper right with a train about to run past it. In the lower left is a wharf from which some of the county's products are placed on board vessels for transport south across Lake Ontario. Bob is currently building a boat for the barren water surface. The main line in the foreground appears to wander into space: in reality there is a hinged span that allows it to continue to the next section of the layout.



major town off one corner of the oval and a short branch line spur to a lumber mill site. The track is level landscaping suggesting changes in elevation.

This NH&HRy is definitely not a class 1 railway. With its deferred maintenance it is akin to a class 4 or class 5 railway if there is such a classification. Set in the transition era, the locomotives are second or third hand be they steam or diesel in keeping with the company motto, inexpensive is good. Rolling stock, if company owned, retains its original reporting marks, painting of these items and engines considered a frivolous expense.

Track power is DC from Tech II throttles, one at each end of the line, Harper's Landing and Terrance. A third throttle is a tethered remote control unit that allows for mainline control. Track is blocked for operational control using double pole double throw switches.



**Photo Above:** A view of the turntable and two stall engine house that is part of the Harper's Landing rail facility. An atlas turntable with the gears removed allows locomotives to be turned by the finger method as opposed to the Armstrong method as per the real situation. Fingers grip the 'post' extending from the turntable at the right of the engine's front. The mainline with its grey ballast roams through the centre of the scene while the town of Oxford Mills, with its combination of 3 dimensional and flat structures appears beyond.

**Photo Right:** Nuegate Landing is not much more than a crossroads community. The locomotive is about to block the main road temporarily. In the background is the single siding to Sedgewick Lumber. To the left is one of the paper structures Bob built from an Internet file. He built a crude cardboard backing and then glued the printed coloured paper onto the basic form.

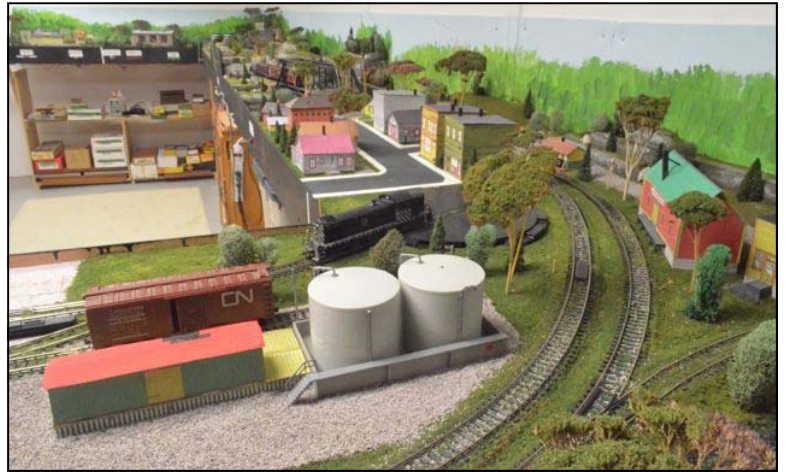
**Photo Below:** At the bridge site more of Bob's handiwork is visible. The riverbank and the hills in the background are all formed from Styrofoam. All have been hand tinted to appear as an appropriate geological formation. The bridge buttresses are formed from plaster and hand carved and painted. Actual seaside pebbles adorn the upper part of the riverbank and one significant rock rests in front of the caboose.



For operation purposes each train completes one full circuit of the rails before arriving at the next sequential town. Car forwarding is achieved by a roll of a die, the first roll determining the town, the second determining which business receives a car. If a car is already on site it is picked up and returned to Harper's Landing which has a connection with the CNR.

To assist operators all towns are readily identified by the name of the community on the station roof. Industries and businesses in each town are similarly identified. When the die is thrown and an industry identified the car type for that location is known and a car is selected from the yard in Harper's Landing.

Trains are short. Only one locomotive per train and no more than five cars per train are the norm. Locomotives come from a variety of sources. Steam locomotives include one Bachmann 2-6-0, one Roundhouse 2-6-0 and one Mantua 0-6-0. Several diesels appear on the layout as well: one Kato RS-1, one Atlas S-4 and one Bachmann 44 ton switcher. Many pieces of rolling stock are from pre World War II manufacturers while newer post war products from Athearn and Roundhouse have made an appearance.



**Photo Above Right:** The way freight is heading south towards Terrance. The bridge is a Central Valley kit crossing the fast flowing Neugate River. The tell tale in the foreground marks the approach to a former wooden pedestrian bridge across the tracks between the two rock faces. In the background a second bridge allows the tracks to cross the canal to the Sedgewick Lumber Company in Nuegate Landing.

**Photo Middle Right:** The Terrance local is easing into Kintail Mills. In the background is the residential section of the community. To the right on a different part of the main line is a siding to a coal dealer and a feed and seed distributor and Wellington Mills, a feed mill. The structures in Kintail Mills and the shed associated with the Terrance fuel dealer are all paper designs.

**Photo Below:** On a shelf above the current HO layout is a small remnant of his original N scale railway, the Prince Edward County Railway. This was the bridge over the Trent River at Trenton with two small Bob built steamers plying the waterway.



# “ARRIVALS AND DEPARTURES THE CPR IN GREY COUNTY”

ARTICLE BY JOHN JOHNSTON

FROM AN ARTICLE BY TOM HAKALA IN THE ESCARPMENT MAGAZINE

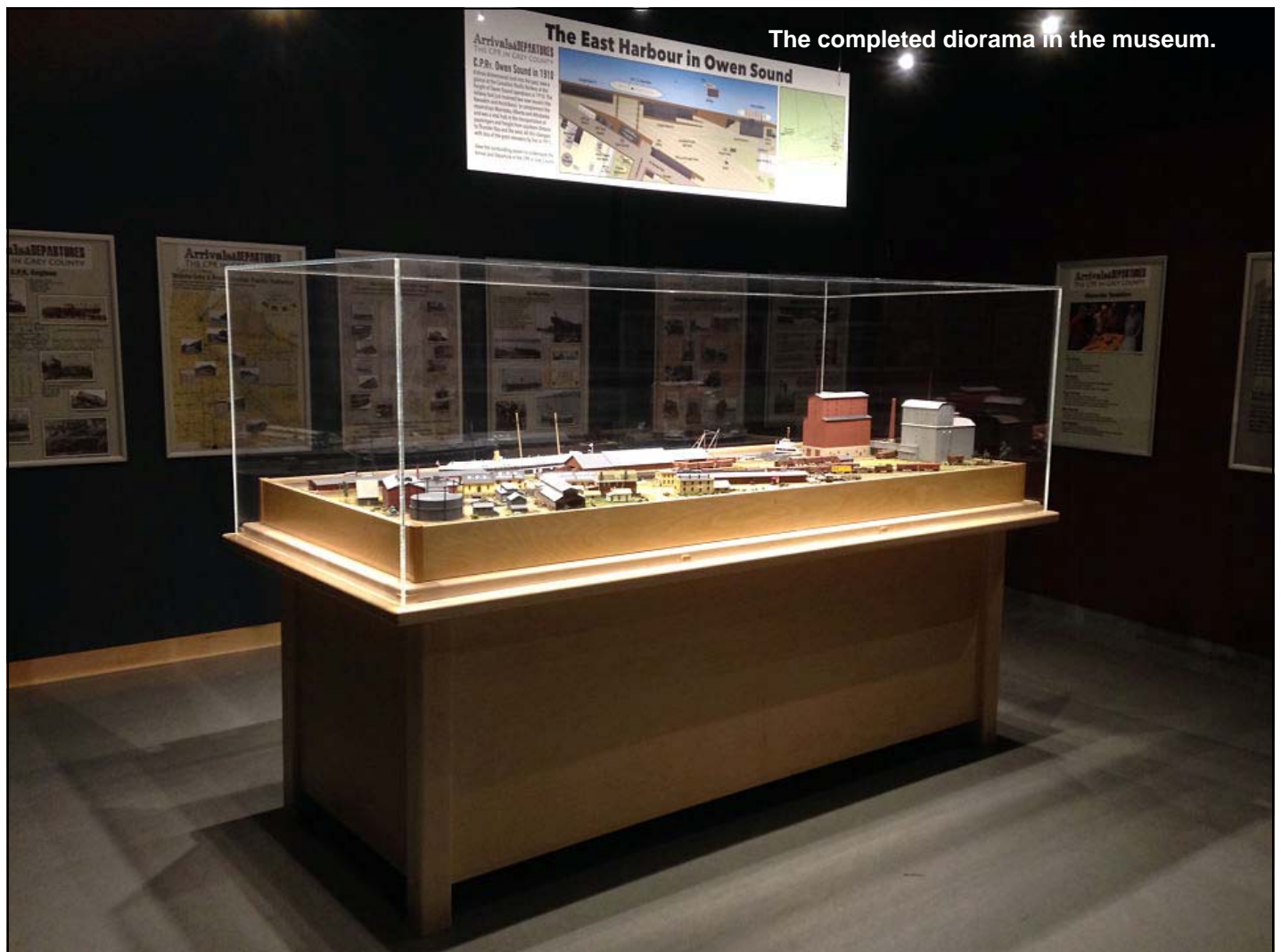
PHOTOS BY TOM HAKALA

We live in a generation that has grown up with the St. Lawrence Seaway, including the Welland Canal, and the large ships that ply the Great Lakes. We forget that the St. Lawrence Seaway opened in 1959 giving ocean going ships access to the Lakes, and the Welland Canal in its present deep draft wasn't opened till 1935. This meant that during the late 19<sup>th</sup> and early 20<sup>th</sup> centuries towns on Lake Huron, such as Goderich, Collingwood, and Owen Sound were important ports and transshipment points between boats and rail for both goods and passengers.

The Toronto Grey and Bruce Railway operated a narrow gauge railway to Owen Sound until conversion to standard gauge in the 1880's and purchase by Canadian Pacific shortly thereafter. Owen Sound became a “hub” for

CP passengers travelling from Montreal and Toronto to points west. They would travel by rail to Owen Sound and then continue to the Lakehead on CP Ships. Similarly grain moved in the opposite direction, and CP opened a large grain elevator on the east side of the Owen Sound harbour. Rail traffic into Owen Sound boomed until 1910/1911 when two events overtook the port. The CP grain elevator burned down on December 11<sup>th</sup>, 1911, and CP opened new facilities at Port McNicol which offered a more direct route to Toronto.

In 2010, a group known as The Bluewater Modellers approached the Grey Roots Museum and Archives to build an N Scale diorama detailing the CPR in Owen Sound as it existed in 1910 at the height of its operations. This



The completed diorama in the museum.





**Photo Above: Clive Morgan working on the diorama**

would be the group's third project, having previously built dioramas for display in Tobermory and Cabot Head. The members of the group include, Clive Morgan, Brian Swanton, Mike Marshall, Stan McClellen, and Tom Hakala.

Bluewater Modellers was formed through a Bruce County initiative to encourage retirees to get together in a productive way. They channelled their many talents into research, model building, and the creation of historical reproductions. To give you a sense of the detail the group brings to its projects, consider the CP grain elevator and the Machine Shop. Attendees at the Museum can see the inside of the elevator, so they need to know what the inside of the building looked like in 1910. Visits were made to old grain elevators to gain insight into their operations. Photos of old machinery typical of the machine shop were found and parts were made using a 3D printer. This level of detail was applied to all of the structures on the module.

Stan McClellen has an extensive background and interest in ships of the Great Lakes so he undertook to build the SS Manitoba. The Manitoba was built in Owen Sound for Canadian Pacific by Polson Iron Works and was the first steel ship built in Canada. Launched on May 4, 1889, she was the largest steamship on the Lakes at the time.

Clive Morgan built the base and handlaid the track. Mike Marshall and Stan McClellen used thousands of pieces of scale wood to create the docks and sidewalks on the module. The docks were stained to give them an aged appearance and the entire group worked on items such as track ballasting and ground cover. All structures are scratch built to specifications from the Owen Sound Insurance map book. The team used Evergreen styrene for the majority of the buildings, while a British made product was used for the brick walls. The boxcars were built from true scale kits and decals were custom ordered from Blackcat Decals.

Clive Morgan and Tom Hakala each kitbashed a locomotive

starting with an N Scale engine and then modifying it to replicate the CPR engines of the turn of the century. Tom found a deal on a large number of people from a supplier in China. They had to be individually repainted into dress appropriate for the time. The group believes adding scale people played a major part in bringing the diorama to life. Wayne Beutel made the other ships on the diorama and Jenna McGuire painted the harbour. All in all, a true team effort.

Discussions with Museum staff led to the decision to come up with a formal name for the diorama and so "Arrivals and Departures – The CPR in Grey County" was born. After 8000 hours of work and 5 years of research, the diorama took its place in the Museum.

This exquisite diorama by the Bluewater Modellers is definitely worth seeing. You can see it at the Grey Roots Museum and Archives, 102599 Grey Road 18, Georgian Bluffs, which is just south of Owen Sound.



**Photo Above and Below: These two photos show the comparison between Owen Sound Harbour in 1910 (photo above) and the diorama (photo below). Both photos show the Iron Works.**



We can see the hotel and station at the harbourfront in Owen Sound



The group looks over the diorama



# PUBLICATION SCHEDULE FOR *THE CANADIAN*

*The Canadian* is published four times per year.  
Submission by authors or Chapters should be submitted  
by the following dates.

Spring Issue: February 1      Summer Issue: May 1  
Fall Issue: August 1      Winter Issue: November 1

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# PROTOTYPE AND MODEL BALLAST

## PART 2: THE MODEL

ARTICLE AND PHOTOS BY GERALD HARPER

In Part 1 of this article I reviewed ballast styles and characteristics displayed on operating railroads. I hope it was helpful in allowing you to decide on what type of ballasted appearance(s) you want for your model railroad. Assuming you have main lines, branch lines and yard tracks it is quite likely that you will want two or three types of ballasted appearance. So the first step will be acquiring the ballast material suitable for each of the track needs. There are several ways to acquire the right material. The most expensive way (for a large layout) is to buy plastic jars of ballast from the commercial hobby manufacturers such as Woodland Scenics but it has the advantage it comes in a range of colours and in the correct size range for the scale. The second way is to be lucky at a flea market model railroad show and find someone who has prepared jars of ballast grit similar to the commercial products. However these suppliers probably have a smaller range of colours and you must be convinced that the product does not contain any clay which will make a mess during glueing. The third method is to make your own, which is what I do and which allows the greatest flexibility in terms of sizing, colour selection and blending.



**Photo 1 Above: Commercially available 4 inch diameter sieves and catch pan**

To make your own ballast your first and most important need is a set of sieves. The ideal sieve set nests with a collection pot at the bottom and a lid so that when you shake them, dust does not go everywhere. Otherwise do it outdoors and wear a mask. Figure 1 shows a commercially available set of sieves. Commercial sieve sets come in either 4 inch or 9 inch diameter. The former are usually plastic and the latter brass. They can be purchased at stores that sell laboratory and/or mining exploration supplies. I purchased mine from a store in Mississauga but the web will help you find the supplier nearest you. The sieves come in multiple mesh

sizes but you can limit your selection to the sizes you need for your ballast as shown in Table 1. You will need two sieves for each size fraction you want to make so that oversize and undersize are separated.

Therefore mainline ballast with a size of plus minus 2 – 3 inches would require material passing 10 and trapped by 20 mesh sieves for O scale, 20 – 40 mesh for HO scale and 40 – 80 mesh for N scale.

If you do not want to invest in a set of sieves you can try

**Table 1 Mesh sizes converted to ballast diameter for O, HO and N scale track.**

Sieve mesh size and cross sectional dimension of opening in inches	O scale (inches) max particle size passing through sieve	HO scale (inches) max particle size passing through sieve	N scale (inches) max particle size passing through sieve
>10	>3.2	>5.8	>10.7
10 (0.0661)	3.2	5.8	10.7
20 (0.0331)	1.6	2.9	5.3
40 (0.0165)	0.8	1.4	2.6
80 (0.0070)	0.3	0.6	1.1

and find some cheap kitchen sieves and use them instead. The fourth column on the right in Table 1 gives the cross sectional dimension of each mesh size so you can measure your kitchen sieve and match it with mesh size. Keep in mind that only particles that are slightly smaller than the mesh opening will pass through the hole and that rounded grains will pass through more easily than long particles which may have one dimension greater than the mesh opening. The more you shake the sieve set the more grit will pass through the sieves.

Having got your set of sieves where do you get the material to sieve? There are many sources and of course the critical element in selection of the material is its colour. Shape is also important as one wants grains with all three axes similar in length. A few examples of materials which work well and are cheap follow.

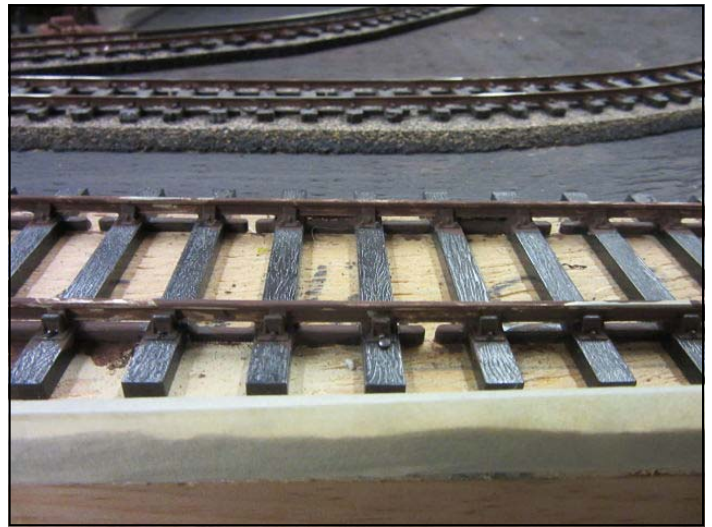
- Limestone screenings, which are used to underlay stone paving around houses and are available from garden centres in 50 kilogram bags or larger.
- The coarser fraction accumulating after a rain storm in ditches, at the bottom of eaves trough down spouts, gravel in ditches, sand and grit from construction areas run-off.
- Roofing supply stores sell bags of grit for every colour of shingles that are sold so that roofers can patch up areas. The shingles colour range provides everything from very realistic red slag to dark green trap rock. The only restriction is that the grit size is too large for N scale and less than mainline HO scale unless you attempt to crush it finer before screening.

If you pick up natural material from ditches or elsewhere make sure it is totally dry before attempting to sieve it otherwise you will just end up with clogged sieves. Clean the sieve by turning it upside down and brushing it with a camel hair paint brush or blowing compressed air through it. The material should also be clay free so that it doesn't dissolve into a slurp when wetted with glue. So do not use "kitty Litter" type products which deliberately have a clay content to soak up moisture. The best way to test for clay is to touch the material with your tongue. If there is any clay present the material will stick to your tongue.

So you now have a line of large coffee cans on a shelf in your layout room labeled with the ballast colour and grain size they hold and you are ready to start ballasting your track. The first step is to paint your track so that it has the appropriate rusty sided appearance as in Figure 2. When you paint your track don't worry if the paint spills over onto the top of the rail and do not clean it off as the paint will protect the running surface of the track from the glue that will spray everywhere during the ballasting process. The final clean up will be easy if you are scraping the paint overlain by glue off the rail.

In Photo 2 note the strip of masking tape placed along the edge of the module at the approximate position for the limit of the ballast from the track. Placing masking tape strips either side of the track helps immensely to control the ballast distribution.

The most common method of ballasting is to hold the



**Photo 2 Above: O and On30 gauge tracks painted with "coffee" brown acrylic paint prior to ballasting.**

rock chips in place with diluted white glue to which one or two drops of liquid soap or detergent have been added to reduce the surface tension effects of the water which prevents it spreading over all the ballast grains. My own experience is that this mixture of glue, water and detergent works very well except that it does not spray well out of a spray bottle as suggested as the preferred method of application. If anyone knows of a way of making spray bottle application work to create a misting spray please let me know. Bachmann sells (or used to sell) packets of powder that one mixed with water to make a paste and then added detergent. I have not tried it and cannot comment as whether it is superior to white glue.

My own preferred method is to make up some dilute glue and paint it onto the strip to be ballasted, between the masking tape and then spread ballast thereon with some sort of a spreading device. They can be homemade or bought commercially. Photo 3 shows an example of such a device, spreading ballast. This device has dis-

**Photo 3 Below: Ballast spreading**



charge holes that are too small for use in ballasting O scale track but is fine for HO and smaller. After applying sufficient ballast to give the desired profile, then I saturate the ballast again with diluted glue applied from a dropper bottle. This works well, doesn't clog up and doesn't blow the ballast away as can happen with a spray bottle. Leave the ballast to dry for at least 24 hours and then make a second dropper bottle application to fix the areas missed the first time. Wash out the dropper part of the bottle after usage with warm water.

Hardware and Do It Yourself stores sell white glue in 1 – 2 litre bottles as well as the smaller dispensing containers. I top up my smaller bottles from a large bottle and mix considerable quantities of diluted glue for ballasting which I store in a clear plastic bottle.

The glued ballast will usually end up a slightly darker colour than the rock chips appear prior to application. Therefore if striving for a specific colour keep this darkening effect in mind. Special colour effects can be achieved either by mixing two or more colours and types of grit before application or by adding a second colour as a superficial layer on top of the base ballast layer. Figure 4 shows the ballast on my HO layout where the track emerges from the coal flood loader and spillage occurs onto the track. Photos 4 – 7 show other examples of ballasted track appearance.

Photo 7 shows ballasted track approaching a switch. Some modelers ballast right up to and through a switch. My preference is not to do so as it can very easily interfere with the operation of the switch, particularly if it is a powered switch with the motor underneath. I use copious amounts of masking tape to protect the switch areas that I do not want either ballasted or glued. Put some plastic bags or coffee cans in your car trunk so that you are ready to scoop up some grit anytime you see some on the roadside and you will be surprised how quickly you have a stock of sievable grit. Then you will be able to get going with ballasting and appreciate how it quickly gives your trackage a much more prototypical colour.



**Photo 4 Above: Dirty ballast on a spur leading into a scarp yard. Photo 5 Below: Mainline track in a mountainous area with a talus slide close to the track. It has been laid with concrete ties and fresh ballast which is not appreciated by a resident moose as the new ballast buried the grain spills.**



**Photo 6 Below Left: Ballasted track emerging from a coal loader displaying considerable spilled coal contaminating the ballast appearance. Photo 7 Below Right: Ballasted track leading into a switch.**





# EVENTS AND TRAIN SHOWS

**Paris, ON:** Sunday, January 15th, 2017, 2017 Paris Junction Model Train Show, Sponsored by The Western Ontario Division - NFR – NMRA. Featuring Operating Layouts, Vendors and NMRA Craftsmen tables. Paris Fairgrounds, 139 Silver Street, Paris Ontario. Hours 10 am, to 3 p.m. Admission: General \$4.00; NMRA WOD Members \$3.00; Children under age 12 Free. For more information contact Edward Howes, Train Show Coordinator at 519-442-7193 or email: [ehowes@boardresources.com](mailto:ehowes@boardresources.com)

**Port Hope, ON:** 24th Port Hope Model Railway Show, Saturday, February 4, 2017 @ 10:00 AM — Sunday, February 5, 2017 @ 3:00 PM Sat, Feb 4, 2017 @ 10:00 AM — Sun, Feb 5, 2017 @ 3:00 PM. Town Park Recreation Centre, 62 McCaul St. , Port Hope, Ontario. McCaul St. is on the east side of the Ganaraska River off Elgin St. Sat 10 to 4.30 Sun 10 am to 3 pm Adults: \$5, Seniors \$4, Children 6-12 \$2, under 5 free. 2 day pass available. <http://www.porthopetrainclub.com> [gana-raska.railroaders@gmail.com](mailto:gana-raska.railroaders@gmail.com) Dave: 905.800.0410 Presented by the Ganaraska Railway Modellers

**Barrie, ON:** 47th Barrie Allandale Model Train Show, Saturday, February 18, 2017 @ 10:00 AM — Sunday, February 19, 2017 @ 4:00 PM Sat, Feb 18, 2017 @ 10:00 AM — Sun, Feb 19, 2017 @ 4:00 PM. Bradford Greenhouses Garden Galleria, County Road 90 at County Rd. 27, Barrie, Ontario. The Barrie-Allandale Railway Modellers Exit 96 off Hwy 400 and go west. Sat 10 to 5 Sun 10 to 4 Adults \$7, Seniors/Students \$5, under 6 free. Sponsored by NMRA.

**Essex, ON:** Saturday, February 25th and Sunday, February 26th, 2017, Essex Model Train Show, Essex Public School. 72 Brien Street East, Essex. Hours: 9:30 a.m. to 3:30 p.m. (both days) Admission: Adults \$5.00 (each day); Seniors and Teens \$4.00 (each day), Children \$2.00 (each day); Ages 3 and under Free For more information call 519-776-9800 or email: [heritageessex@bellnet.ca](mailto:heritageessex@bellnet.ca)

**Cobourg, ON:** Cobourg Model Train Show, Saturday, March 4, 2017 @ 10:00 AM — 4:00 PM Sat, Mar 4, 2017 @ 10:00 AM — 4:00 PM . Cobourg Lions Community Centre, 157 Elgin Street E., Cobourg, Ontario. Between Division and D'Arcy Sts. From 401, take exit 474 south (Division St.) to Elgin and turn left/east onto Elgin. Just past Fire Hall on south side, parking in rear of Community Centre. Vendors, operating layouts, exhibits. Food and beverages. Adults \$5, Children 12 and under \$1. Sponsored by the Cobourg Model Railroaders. Information: Ted Rafuse: 905.372.8375 or [tedrafuse1@gmail.com](mailto:tedrafuse1@gmail.com)

**Copetown, ON:** The Copetown Train Show, Sunday, March 5, 2017 @ 10:00 AM — 3:00 PM Sun, Mar 5, 2017 @ 10:00 AM — 3:00 PM. The Copetown & District Community Centre, 1950 Governor's Rd., Copetown, Ontario. The Copetown Show Just east of Hwy. 52 10 to 3.30. Admission \$6, under 12 free. Operating Canadian prototype layouts. Displays by model craftsmen. Railway Special Interest Groups. Canadian Model Manufacturers Displays and Sales. Historical Societies, Scratchbuilding Supplies, Manufacturers, Craftsman Displays, Slide and Photo Collections, and much more for the serious modeler. Refreshments available on site. Contact: Steve Tuff Email: [copetownshow@yahoo.com](mailto:copetownshow@yahoo.com) or [www.facebook.com/CopetownShow](http://www.facebook.com/CopetownShow) Presented by the Bayview Modular Group, sponsored by Rapido Trains Inc. and the Canadian Association of Railway Modellers

**Kingston, ON:** 28th Kingston Rail O Rama Model Train Show, Saturday, March 11, 2017 @ 10:00 AM — Sunday, March 12, 2017 @ 4:00 PM Sat, Mar 11, 2017 @ 10:00 AM — Sun, Mar 12, 2017 @ 4:00 PM. Ambassador Hotel, 1550 Princess Street, Kingston, Ont. 10 am to 4 pm both days. Free parking, wheelchair access., refreshments, dining available on site. Operating layouts, Vendors, Displays, Thomas Activity Table. Adults (13+) \$6, Seniors (60+) \$5, children 5-12 \$3. Presented by the Canadian Historical Railroad Association (Kingston Div.) with the cooperation of the Ambassador Conference Resort [www.kingstonrailorama.wix.com/show](http://www.kingstonrailorama.wix.com/show)

**Bowmanville, ON:** Saturday March 11th 2017, 8th Annual Bowmanville Model Train Flea Market. Sponsored by Soper Valley Model Railroad Association. Hours are from 10am to 2pm. General Admission \$5 at the door, kids 12 and under free! Located at Clarington Central Secondary School, 200 Clarington Blvd L1C5N8. Easy to find just off hwy#2 Bowmanville behind Home Depot. Free parking and fully wheelchair accessible! Vendor setup at 8am open to public at 10am sharp! For Vendor and other inquiries please contact Bill Harford Jr through Email: [sopervalley@gmail.com](mailto:sopervalley@gmail.com) or facebook: Soper Valley Model Railroad Association

**Cambridge, ON:** 34th Annual Double Headers Self-guided Layout Tour, Saturday, March 25, 2017 @ 9:00 AM — 9:00 PMSat, Mar 25, 2017 @ 9:00 AM — 9:00 PM Kitchener - Waterloo - Cambridge – Guelph. Over 40 layouts. Registration, tickets and maps 9 am to 3 pm Hespeler Arena 640 Ellis Road West, Cambridge, Ontario GPS: N43.4280 W80.2919. Sponsored by the Doubleheaders Model R. R. Club. Admission \$8, children 12 and under \$5. Preview the layouts on the tour at [www.doubleheaders.org](http://www.doubleheaders.org) Info email [dhtour@gmail.com](mailto:dhtour@gmail.com)

**“ARRIVALS AND DEPARTURES  
THE CPR IN GREY COUNTY”  
PHOTOS BY TOM HAKALA  
“YES, THIS IS N SCALE”**

**Grain Elevator**



**SS Montreal**

