



# THE “CANADIAN”

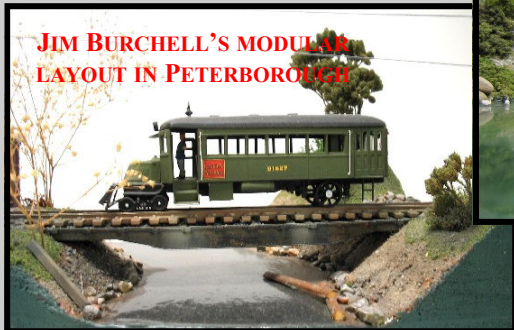
a bi-monthly publication of the  
“Canadian Association of Railway Modellers”  
[www.caorm.org](http://www.caorm.org)

JULY AUGUST 2005 ISSUE #10

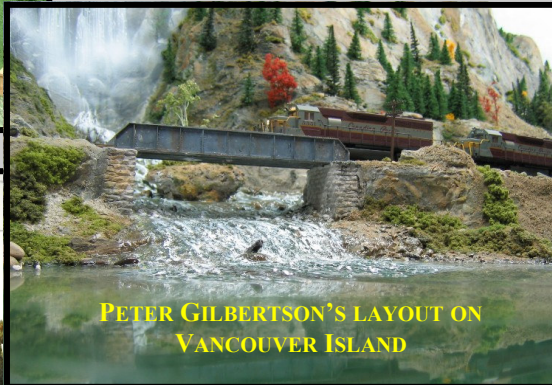
## WINNIPEG A HUGE SUCCESS! MONTREAL IN 2006



HANK DUIVENBODES “S” SCALE  
SOUTHERN ONTARIO LAYOUT



JIM BURCHELL'S MODULAR  
LAYOUT IN PETERBOROUGH



PETER GILBERTSON'S LAYOUT ON  
VANCOUVER ISLAND

Winnipeg is over, but what a huge success it was. On behalf of everyone of us who attended I want to thank John Longhurst and his team in Winnipeg for the great job that they did hosting us. There were many highlights. The sunny and hot railfanning day at Portage la Prairie was well attended. The CP Shop staff were informative and welcoming. Everyone who opened their homes and layouts to us merit our thanks. Ian Wilson gave an excellent after dinner presentation with his insightful look into the last days of CN steam. The Train Show and especially the modular layouts, were a welcome addition to the convention and enjoyed by everyone. The people who took the time to bring out models for display and the clinicians who gave us their time and expertise. Great job everyone!

## MONTREAL 2006

If you are the type who likes to plan ahead, then get ready for Montreal in 2006. The organizing committee is hard at work to prepare a great program for you centered around Exporail. You should start seeing details in the next issue of the Canadian, stay tuned.



MIKE BARONE'S CUSTOM PAINTED DISPLAY MODELS



## THE OBSERVATION PLATFORM

I thoroughly enjoyed my trip to Winnipeg for Golden Rails 2005. With three of my fellow club members I railfanned across Northern Ontario. It has been almost two decades since my last trip across the top of Superior. It was a great opportunity to see Canadian railroading at its finest and I will be preparing an article and sharing photos with you of the CP, CN, Huron Central, and Ontario Northland. On the Ontario Northland we had one of those unique adventures which seem to happen every once in a while. We were staying at the Best Western in Cochrane, good rates and a great view of the station and the yard, and we had walked across to the Station to catch the action. In the yard we saw that the switch crew had put 5 gondolas on the ground. This blocked the cars which were due to go out to Moosonee the next morning on the Polar Bear Express so everyone was working hard to get it resolved.

A GP9/GP40-2/GP38-2/GP38-2 lashup came over from the engine shops to couple on and try to pull the loaded 100 ton gons clear. The engines were idling in front of the station and we were just watching when the engineer shouted down an invitation to climb up in the cab and join him. It didn't take us long to get up there. Over the next half hour we had an in depth look at all four units. The GP9 certainly had its share of miles on it.



I also want to thank Frank Gerry and the guys at the Thunder Bay Model Railroad Club who came down to open their layout so we could visit it the night we stayed in Thunder Bay. They are open during the summer in the old station on the waterfront. They are doing a great job and have many wonderful scratchbuilt structures, if you are in the area try to get down there and visit them. They would appreciate your support and you will see an interesting model railroad.

Finally, a word to all of you who took the time to come up and either provide constructive criticism, or thankyou's for the work we are doing with CARM, the newsletter, and the website. All of your input is really appreciated, it helps make us a better organization. This issue I am publishing the first Letters to the Editor. I am hoping to receive many more. Two of these letters raise issues which merit responses so you will find my responses posted as Chairman. The other letters are from members who attended Golden Rails 2005 in Winnipeg and speak for themselves.

**John Johnston: Editor**

### PASSWORD CHANGE

#### **MEMBERS AREA: EFFECTIVE JULY 5**

**USERNAME: *steam***

**PASSWORD: *ash***

### COVER PHOTOS

**Upper Left Photo: John Johnston:** CN freight passes roadside hot dog stand on Hank Duivenbode's layout.

**Middle Photo: Ed Warren:** CP freight on Peter Gilbertson's layout.

**Centre Photo: Ted Rafuse:** Modified CNR galloping gander crosses a bridge made from an old flatcar.

**Lower Right Photo: John Johnston:** Mike Barone painted these four brass steam engines to display them at Golden Rails 2005. They will be repainted into standard paint schemes after the convention.

### BACK ISSUES AVAILABLE

Back issues of the Canadian are available and may be purchased for the cost of production and mailing. The costs are:

**Canadian Members:** 1 Issue—\$2.75 Each additional issue \$2.00

**U.S. Members:** 1 Issue—\$3.50 Each additional issue \$2.50

**European Members:** 1 issue—\$5.50 Each additional issue \$4.00

Costs are in Canadian dollars, the increased costs for U.S. and European members reflect the increased costs of mailing.

**TO ORDER BACK ISSUES CONTACT THE EDITOR**

*Material for the Canadian should be sent to:*

**Editor: John Johnston, 41 Glenview Place, Hamilton, Ontario, L9C 6H9 or by e-mail at [editor@caorm.org](mailto:editor@caorm.org)**

## LETTERS TO THE EDITOR

### Electing Regional Officers:

Let me begin by saying how glad I am being a member of a Canadian group that looks like it will continue to grow and help model railroaders here in Canada. I would also like to thank you for your encouragement to put my thoughts down and send them to you.

I wonder if the current BOD has given any thought to expanding the board to include members from all areas or provinces in our great nation. I believe that representation from all areas will build a stronger association and hopefully stop the stigma of being an eastern run group out of CARM. I understand cost is a factor and that CARM runs on a small budget along with our members who would rather have no political structure. But what better way to say that CARM is a Canadian organization than to have voices from all modellers across the country. I know that we already have regional reps, but I question what kind of voice they have and I wonder how decisions are currently made at the BOD table and if you do consider the other parts of Canada while making decisions.

In closing, I reiterate that I want to see CARM grow and prosper and become a coast to coast organization promoting a great hobby.

**Ron Adams, Saskatoon**

### Chairman's Response:

Ron, thank you for taking the time to share your thoughts with us. As a general premise we agree with the concept of broadening representation on the Board. In fact, this matter has been under discussion for some time. There are a number of issues which have precluded us moving forward till now.

First and foremost our entire attention has been on getting CARM started and achieving our initial goals of growing member services, publicity and promotion, and adding membership.

Secondly, the membership has been heard by all of us, loudly and clearly, that they want to keep the politics out of the hobby and out of CARM. We felt that part of the solution to this problem was to keep the National Executive small with specific functions and create local autonomous Chapters. We were attempting to create a bottom up organization with a small national leadership. Ensuring representation on the National Board from all areas of the country would mean a national leadership in the 8 to 12 person range. Without arguing that it is good or bad, I simply point out that a group that size will be political, it is human nature. Our approach has perhaps been a little too idealistic and a more broadly structured national organization is required.

**In that context we will be preparing a referendum question to be included with the next issue of The Canadian. This will allow individual members to express their views on the subject. If the decision is to broaden the Board on a Regional basis we will then move forward through a nomination and election process.**

Finally, I would like to put this east/west thing to rest. I have traveled all of North America, east, west, north, and south, meeting modellers wherever I have gone. In 35 years I have never heard it said that modellers in the west do anything different from modellers in the south, or those in the east from those in the north. From my perspec-

ive, this is a non issue in the hobby. The reason our leadership is from the east is because four people had the vision to create an organization for Canadian modellers. The fact that they all live in a specific geographic area is not indicative of anything other than their place of residence. I for one would be thrilled if someone from Halifax, or Thunder Bay, or Saskatoon, or Vancouver wanted to take over the reins in 2006 or 2009. I am not suggesting I am ready to step down, it is just that it would be a natural and mature evolution of the organization. If we are to be successful in focusing on the hobby and not the politics we have to get past these issues, of age, gender, where someone lives, and what scale or gauge they model in.

In closing, let me thank you again Ron for having the courage to bring these issues out into the open.

**John Johnston, Chair**

### Golden Rails 2005

Hats off to John Longhurst and his committee. They put together a very good convention. Clinics were quite good and well attended. Some of the best home layouts I have ever seen were on the tour. It was nice to meet many of you that I have only met on the Yahoo group. A job well done to all!

**Grant Halkyard, Regina**

The Convention was fantastic! From the excellent clinics to the great layouts you Winnipeg guys are building, to the hot, hot, trip to Portage la Prairie, to the wonderful barbecue at the AVR. I saw licence plates from Quebec, BC, Ontario, Manitoba, and Minnesota and met many wonderful new friends in the model train hobby. Thank you everyone for a super convention.

**Ian Maynard, Toronto**

<p><b>2006 CARM CALENDAR</b></p> <p><b>THERE IS STILL TIME TO SUBMIT A PHOTO</b></p> <p><b>Jpeg format 300 dpi or better</b> <b>Or</b> <b>Original colour photo or slide</b></p> <p><b>Include a short description and the name of the photographer</b></p> <p><b>SEND MODEL OR PROTOTYPE SHOTS FOR CONSIDERATION,</b> Peter Moffett Secretary, CARM 7 Jolie Court, St. Catharine's, ON L2M 6V5</p> <p><b>DEADLINE: AUGUST 31</b></p>
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## LETTERS TO THE EDITOR

### CARM's newsletter "The Canadian"

Firstly, thank you and your crew and the other SIGs for a brilliant convention in Winnipeg. I enjoyed every minute from chat in the morning at breakfast to chat at lunch to ... well, you get the idea. It was well organized, good content and had a most pleasant and competent staff from CMU.

Secondly, several points were raised at the CARM meeting on Sunday. In this letter I would like to dwell on the question of what is the role of the newsletter ... is it a bulletin, newsletter or magazine?

I belong to several model engineering clubs which produce a one page newsletter. They always list the next few meeting dates, the speakers and their subjects at the next meeting, the speakers and subjects at the last meeting, the membership wants, needs, for free and for sale, new members and, annually, a membership list.

For a couple of years, when I was editor, I tried to expand the newsletter to two and even four pages including such items as web chat, an article, a cartoon, masthead, cover photo and other editorial content. I started writing articles on EDM. I was then reminded quite curtly by the membership that this was a newsletter and that they subscribed to magazines for the other stuff. Having given it much thought, I have to agree.

It is not the role of the "Canadian" to compete with the regular trade magazines for editorial content. For the "Canadian" to publish even short articles is to deny magazines, like CRM, material which reach a larger readership. Many good ideas and articles surface in local newsletters only to be lost to the general readership due to regional circulation. This is where the NMRA bulletin, "Scale Rails", has gone off the rails. It lost its way as a bulletin of standards and events and became a magazine under overenthusiastic wannabe editors, competing with and diluting content of the large circulation magazines.

The best thing the "Canadian" can do is follow the mission statement of CARM:

- ...to inspire.
- ...to foster fellowship among railway modellers throughout Canada.
- ...to provide a forum for the exchange of ideas and skills between railway modellers.
- ...to promote the hobby of railway modelling in Canada.
- ...to be inclusive of all scales and interests.
- ...to work in co-operation with other organizations.

to paraphrase this:

- ... promote CARM
- ... encourage chapters and fellowship
- ... promote membership and modelling by attending and organizing shows
- ... foster communication between members by publishing letters
- ... list events comprehensively

Much of this is not done by the regular magazines or is done reluctantly.

One item which the Canadian should pursue and which perhaps straddles the editorial content line is the coverage of members layouts. This can be done without expanding to multiple pages. Publishing short letters with a couple of photos would encourage members to write more detailed articles for the major magazines.

**Robert P. Langlois, Port Stanley**

### Chairman's Response:

Again, my thanks to Robert for sharing his thoughts. It gives me the opportunity to share with you my vision of the newsletter and its future.

The role of the Canadian is to provide information about CARM, CARM Chapters, and CARM members to the membership of CARM. The role of the commercial press is to make a profit by publishing information to a specific audience. As Robert has pointed out, the key is where does one end and the other begin.

I see the Canadian's role as:

Keeping the membership informed about official news at the National and Chapter level. This would include Board decisions, financial information, events, member services, etc. This communications role is essential in an organization as spread out across our great country as CARM is.

Showcase the layouts and models of our members. Robert has paraphrased the term "to inspire", to mean "promote CARM". On the contrary, "to inspire" means to encourage members to build better models, to improve their skills, to try new techniques, in other words, to challenge themselves. The Canadian can do this by showing what members have accomplished. In particular, when the CRM program is in full swing we can show what our members are achieving in order to receive achievement awards. How does this differ from the commercial press. The reality is that most of our layouts and models will never be in the commercial press, and our requirements are considerably less for items such as photographic quality. As Robert points out, since our circulation is so much smaller than the commercial magazines we are not harming their circulation and perhaps they may see a layout which perks their interest.

The two other series which we run regularly are Railfanning, and Canadian Authors. Promoting Canadian authors who are trying to preserve the history of our railways in published form is in my view consistent with our goals. The Railfanning series allows our members to share some of their prototype photos since most of our members model Canadian prototypes and give people from out of the area some insight into the best locations from a "locals" perspective.

In Winnipeg, I received the suggestion to start a Buy/Sell section between members. If you think that is a good idea or you have other thoughts to share, please forward them to me.

**John Johnston, Chair/Editor**

# AN “S” SCALE LAYOUT WITH A DUTCH FLAIR

Hank Duivenbode’s Layout: Article and Photos by John Johnston

A trip to Hank Duivenbode’s layout offers you the opportunity to witness both a unique scale, “S”, and a unique approach, modelling Canada and Holland on the same layout. Hank originally was planning an “O” scale layout but felt he didn’t have the room and while at a Train Show in Toronto discovered “S” and decided it was the scale for him.

The layout is in the shape of an E. One leg is clearly southern Ontario, the other two legs have scenery which shows off Hank’s Dutch heritage complete with windmills and canals. Hank uses Code 100 rail throughout, tortoise slow motion switch machines, an operating signal system, Digitrax DCC and sound, and the layout is populated with over 300 figures and almost 200 vehicles. Many of the buildings on the layout are scratchbuilt out of cardstock placed over plywood frames.

Hank runs both CN and CP on the layout and the roster is mostly steam with one steamer. Much of the rolling stock is scratchbuilt and is a blend of Canadian and the odd piece of Dutch.



Photo 1: Above: CN Geep works the yard.

Photo 2: Below Left: CP passenger train led by an FA crosses the scratchbuilt trestle at the front of the lay-



The layout is equipped with a great deal of animation and is fully lit. The lights can be dimmed to create a night scene as all of the buildings have lighting. The windmill’s arms rotate and the restaurant on top of the needle tower rotates to give diners a view of the entire layout. There is both an operating transfer table and a turntable. The canal which runs down one leg of the layout uses real water which is recycled through a pump.

The layout is full of super detailed scenes such as men building a deck, a children’s playground, a hot dog stand, all of which have been scratchbuilt. For the future Hank is looking to more superdetailing, adding to his roster, and putting operating crossing signals on his grade crossings. Hank hosts an open house each May and notice of this event should be posted in the Canadian.

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Photo 3: Above: Looking down the main street of the Dutch village with the needle tower and its rotating restaurant in the background.

Photo 5: Right: An overview of the yard.

Photo 7: Below: 72 year old Hank Duivenbode looks over his layout. Hank was born in Holland and is now retired after 25 years working at Schneiders. Both his Dutch heritage and Schneider's can be found on the layout. Before getting into model railroading, Hank's previous hobby was woodworking and he is delighted to show you some of his fine handiwork. This beautiful "S" scale layout was finished in just 5 short years.



Photo 4: Above: CN FA pauses at the station in front of the operating windmill.



Photo 6: Below: Locals purchase a hot dog at the stand as a CN freight rolls by on the mainline.



# CANADIAN AUTHORS CONSTRUCTED IN KINGSTON

Article by Ted Rafuse

Constructed in Kingston is Don McQueen's premier Canadian railway history reference to date. With co-author Bill Thomson this volume chronicles the history, in text and image, of the Canadian Locomotive Companies from 1854 to 1968. Now out of print this book remains a standard reference source in Canadian locomotive railway history.

Those unfamiliar with his past might suspect that Don came to his interest in railways as a student while at Queen's University in Kingston from which he was granted an honours degree in history. This led to a teaching career in London, Ontario, from which he retired several years ago. Retirement has not however prevented Don from continuing to research and to write on the topic of Canadian railway history. Recently he had been working with Ray Corley in a revision of Canadian National Steam Power. With Ray's recent passing, Don will continue the project which is his current consuming interest.

Don's interest in railways however came from a time pre-dating his university days. In fact he has been a railway photographer since 1953. Several years earlier he had been given a Kodak Brownie Hawkeye but it was not until a visit to an uncle that the camera was focused on trains. On a summer farming experience he was told one day of the coming of the noon train on the Kingston & Pembroke Railway. From one edge of the property Don captured mixed #613 headed by CPR D-4-g 424 at Glenvale, Ontario, on August 29, 1953. He was hooked!

Prior to attending Queen's University, Don grew up in Brockville. His interest in railways could be fulfilled as Brockville was a divisional point along the CNR mainline double track between Montreal and Toronto. Less speedy trains, but also of interest could be seen on the CPR Brockville line. More than fifty years later and with some 30,000 images in his files, Don remains a very active photographer of the current railway scene.

Don has contributed many of his images, and writing, to a variety of railway publications. These include Canadian Rail, Branchline, Kingston Rail, Extra 2200 South and CNLines and Mainline Modeller. Don is a charter member of the Forest City Railway Society and has served in many editorial roles for Tempo, the FCRS monthly newsletter.

Look for Don's revision of Canadian National Steam Power shortly. It will be another must have Canadian railway history reference.

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# SCENES FROM GOLDEN RAILS 2005

Photos by John Johnston and Justin Parry



**Photo Top Left:** Ted Rafuse finishes up at his clinic on the Crossen Car Company.

**Photo Top Right:** W. Tokaruk's Schnabel car in the display area.

**Photo Centre Left:** Group getting safety equipment at CP Shops.

**Photo Centre Right:** Portage La Prairie, CN freight with CN and ONR units leading.

**Photo Bottom Left:** Group enjoys riding the trains at the AVR on Friday night.

**Photo Bottom Right:** Winnipeg modular N scale group display their layout at the Train Show.

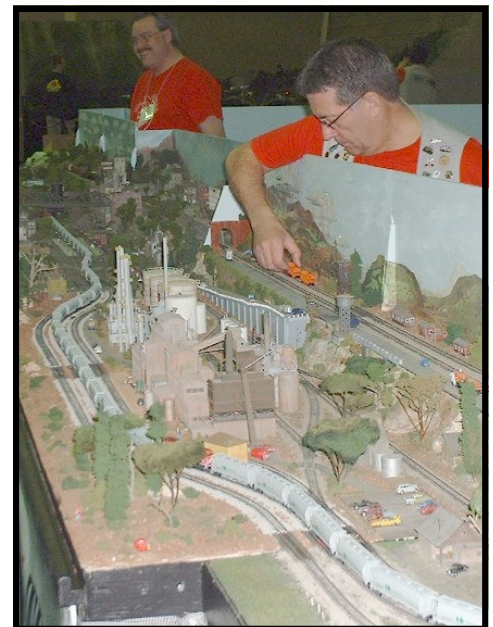






Photo Left: Ed Mulholland's layout

Photo Below: Stafford Swain's layout.

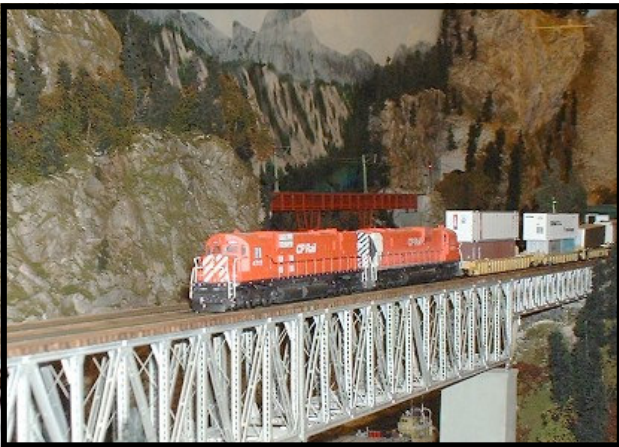


Photo Left: Arnold Walker's layout.



Photo Above: Arnold Walker's layout.



Photo Left: Brian Schuff's layout.


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# FOR A LOVE OF TRAINS

**Jim Burchell's Modular Layout: Article & Photos by Ted Rafuse**

It is difficult to know where to begin Jim Burchell's love story. It is far more than an affair as it has endured many decades. It involves a number of scales. It includes a variety of facets including art and history and imagination. It beckons him to travel the world. It urges him to proclaim his love in many public places.

Jim began his love, as have many others, with a Lionel 027 train set he received in 1956. He was smitten. As a youngster living beside the tracks in Ottawa his interest in trains was reinforced by the daily parade of locomotives and rolling stock that passed his eager and intent eyes. From this vantage point he observed the transition that took place on Canadian railways as the use of steam declined and diesel use ascended.

Two years later an HO Lionel set was obtained and most of that equipment is still in use today as Jim turned his attention to HO modelling as a preference. A train ride across Canada, from Halifax to Vancouver, about this time absolutely confirmed his devotion to trains.

Over the years Jim's interest in trains has led to further travel. He has ridden aboard trains extensively in Canada and the United States as well as in England, Germany, Switzerland, France, New Zealand, and, most recently, in Australia. A camera accompanies him on these distant forays as it does closer to home and consequently he has an extensive collection of images to recall his railway adventures.

Another medium Jim enjoys is sketching stations, a skill at which he demonstrates a fine prowess. In particular he has a portfolio of stations surrounding the Peterborough area, many of which no longer stand. These often find their way onto personal Christmas cards which become keepsakes for the recipients. He is an avid patron of railway books and art and his library reflects this avocation.

Currently Jim models in three scales. The oldest layout is a modular layout built in four sections which occupy a foot print of 6.5 x 9 feet. It is a traditionally constructed layout with 1 by 4" pine ladder frames with 1/2" plywood on top. Atlas code 100 flex track is laid on cork roadbed. All sections are bolted together with trailer automotive connectors



**Photo 1: Not every railway has an operating gin car on its premises. Jim created this whimsical duo from his fertile imagination. On the right is the actual still car which bubbles with brew while on the left on the vat car are the open top storage vats for all to see the clarity of the 'shine.' Jim entered this pair in the NMRA Maple Leaf Convention model show and won first prize.**

**Photo 2: CNR #37465 articulated diesel electric was constructed in the local shop perhaps by a crew that had imbibed the results of the distillery car. The base model for the combine and trailer car were MDC Harriman coach and combined models. Jim refashioned the baggage end of the combine to appear as a diesel electric locomotive. Note the added details at the front of the**



  
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providing an electrical link between modules. Operation is designed for two cabs with Aristocraft Train Engineer throttles. Track is flat but the fascia is cut to provide the illusion that the track runs through varied landscape. Scenery is made of paper mache and coloured using water based paints. As a portable layout that often visited train shows the track plan is essentially two loops designed for continuous and independent train running.

In addition to this HO layout, Jim had a second more modest but more travelled layout. A 3 by 4 foot independent module has visited many venues with Jim close by. This completely operating layout is used for teaching seminars about model railroading and encouraging the general public at train shows to get involved in the hobby. This layout also appeared in the NMRA/Maple Leaf 2003 Train Show in Toronto.

Several years ago Jim became infatuated with garden railroading. After some consultation with his understanding spouse Mary he embarked upon a G-Scale layout in the backyard. The outdoor layout is 6 by 50 feet, essentially a loop with two sidings serving various industries. In another article Jim will describe how this layout evolved as well as how he overcame potential problems. During the winter the garden railway can be operated from the shelter of his home through the use of the previously mentioned Aristocraft throttles. With winter weather restricting the outdoor operation, Jim constructed a 4 by 6 foot portable layout for use indoors and to test his various pieces locomotives and rolling stock.

Jim enjoys scratch building, be it locomotives or rolling stock or structures or vignettes. Everything on his various layouts either tells a story or has a story. His home workshop reflects the tools of this aspect of model railroading. He often acquires the typical sow's ear of a model and through creative imagineering, or faithful implementation, produces a silk purse. With this life long affinity for trains, it is not surprising that Jim is a member of several organizations promoting the hobby. He is a member of the Peterborough Model Railroaders, CARM, the NMRA and Niagara Frontier Division, and the World's Greatest Hobby. At Sir Sandford Fleming College in Peterborough Jim has taught an introductory course to model railroading. Always enthusiastic about his love, he is always ready to communicate with others on the topic of railways.



**Photo 3:** The railway's top hat is Jim Burchell here demonstrating his omnipresent smile. Always willing to share his railway experiences, he is always receptive to being approach to engage in conversation. Hi is both a good listener and a walking source of railway and modelling



**Photo 4:** Diesel-electric #1928 with its trailer coach is ready to depart. Note the early open air touring car in the parking lot in this early twentieth century image.



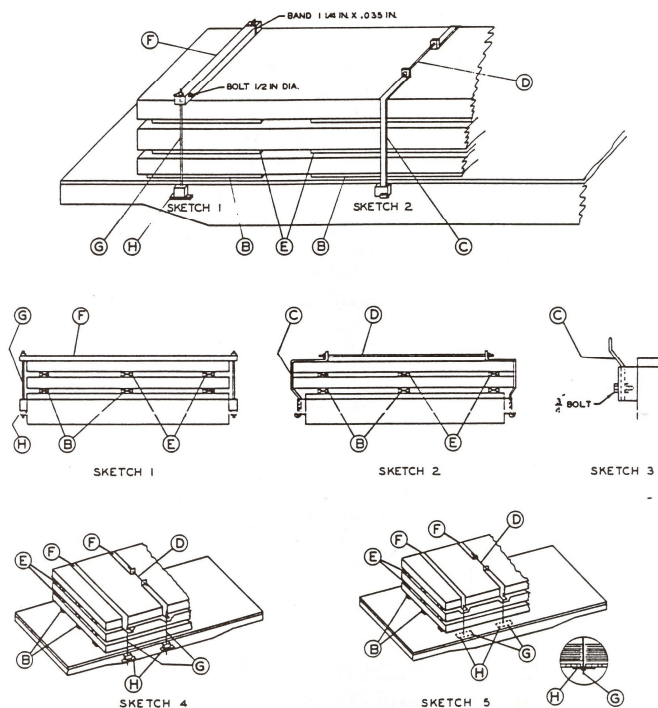
# STEEL MILL SHIPMENTS - PART 3

By Ron Tuff and Ray Hoadley

This issue we take a look at the type of flat car and gondola loads we would see in the steel industry and how they would be secured. Consider the prototype size and weight of a load. Do not overload the car. Use a generous amount of wooden blocking and/or steel banding to secure the load while it is in transit. Wood blocking would be 4X4 scale lumber, steel banding would be black striping. Consider how a load is going to be unloaded. Should it have wooden blocking beneath it so a can get cables or chains under the lift. Do not overweather outgoing steel shipments. They are generally brand new finished products. Next issue we will deal with how steel coils are shipped

## EXAMPLES OF FLAT CAR AND GONDOLA LOADS

Sec. 2—Fig. 14-B (Rev.—11-1978)  
WIDE PLATES & SHEETS—FLAT CARS



Item	No. of Pcs.	Description
A		Brake Wheel Clearance. See Fig. 2, Sec. No. 1.
B	As required.	Bearing Pieces, lumber, minimum 1 in. x 3 in. Crosswise bearing pieces must be one piece, length about equal to width of car. Lengthwise bearing pieces must be minimum of 8 ft. in length. If lengthwise bearing pieces consist of more than one piece, space ends no more than 3 ft. apart. Lengthwise bearing pieces must be placed not less than 6 in. nor more than 12 in. from edge of bottom plate, for plates which do not exceed width of car or from side of car for plates which exceed width of car. Use optional.
C	See Chart.	1/2 in. x 3 1/2 in. steel, length to suit per Sketch 2, locate about 1/4 length but not more than 7 ft. from ends of pile, with intermediate Item "C" equally spaced in between. Form and secure to stake pockets as shown in Sketch 2 or secure to stake pocket using 3/4 in. bolt, washer and nut as shown in Sketch 3.

1978



Photo 1: CN gondola 145238 carrying steel plate



Photo 2: CN gondola 148835 carrying steel blooms



Photo 3: TH&B flatcar 1823 with boxes

Models on this page  
by Ron Tuff



Photo 4: BCR91122 flatcar with lumber



**Photo 5: CNR gondola 155237 loaded with heavy melt. Model by Ron Tuff.**



**Photo 6: ACR gondola 3769 loaded with bundles. Bundles created by pressing aluminum or tin foil into two cubic foot squares. Model by Ray Hoadley.**



**Photo 7: TH&B gondola 2261 loaded with turnings. Commercial scrap loads used. Model by Ray Hoadley.**



**Photo 8: CPR mill gondola loaded with rods. Rods made from steel wire. Model by Ray Hoadley.**



**Photo 9: TH&B 1860 loaded with large plate. Plates made from sheet styrene. Model by Ron Tuff.**

## CHAPTER NEWS

### Vancouver Island Chapter:

On May 7th, 2005, members of the Vancouver Island Chapter assembled for a day of touring some fine local layouts. We gathered at a central location and carpoled to the various locations, eating lunch along the way.

The first stop was at Ed Warren's O scale Esquimalt and Nanaimo Railway, a 20 by 20 foot version of that CPR shortline. The track is handlaid code 100 rail- which matches the 85 lb. prototype- to proto48 standards. The switch stands work the same as the prototype, and are either scratchbuilt from brass sheet and wire, or modified Alder Models (now Surrey Custom Models) static switchstands. The line is modelled from the town of Ladysmith (about 100 km. north of Victoria) to Parksville, where it splits, one branch going to Port Alberni and the other to the town of Courtenay. Because O scale demands a lot of real estate, the action is in switching town industries, not mainline running. There is a short On30 logging line to take advantage of the newer offerings of sound equipped models. The layout is operated using a Digitrax system so the engineer and brakeman can follow their train to its destination. The era modelled is the 1960's, when first generation diesel power was the norm, and the logging roads were still using steam power.

The next stop was the home layout of Andy Barber. It is a large 35' by 14' rectangular HO layout with a 25 by 2 foot wing extending into the garage for staging. There are two levels, cantilevered off the walls with heavy duty brackets, connected with a 60 foot long helix. He models the British Columbia Railway, as it was on May 9th, 1990, from North

**Photo 3: This sawmill, just south of Squamish is one of the online industries providing switching traffic on Andy Barber's layout. A chop nose Alco C420 leads a work train on the mainline.**



**Photo 1: Courtenay station, on Ed Warren's Esquimalt and Nanaimo Railway. Passengers wait for the arrival of the daily passenger train from Victoria. The station is scratchbuilt from styrene, from measurements taken from the prototype.**

**Photo 2: A 2-4-2T Porter pulls an empty skeleton log car across the E&N diamond at CZ tower. The interlocking tower is a scratchbuilt model of the prototype that controlled movements where the Comox Logging Railway crossed the E&N (CPR) main**



Andy says that the date has the advantage of allowing a great variety of motive power. BCR was operating steam excursion service, passenger service with RDC's, and a mix of first, second and third generation diesels in a variety of paint schemes. In North Vancouver tracks lead east to hidden staging tracks, representing the prototype connection to the Canadian National Railway.



**Photo 4: BCR Diesel shops, North Vancouver. On the prototype this was a great trains watching spot, but the shops have been mostly dismantled in the past year since the CNR takeover of BC Rail, and diesel service is now at CN's Thornton Yard.**

The westbound line leaves the large North Van. yard and crosses the Capilano River bridge, a scratch built through truss. It then passes through the tunnel at Horseshoe Bay, and turns north along Howe Sound. Just past the old mine at Britannia Beach, the line enters the helix and rises 18 inches to the second level. The beautifully painted backdrop is very evocative of the scenery along Howe Sound. We pass Squamish station and the harbour, and then enter the small yard at Squamish, with the BCR car and engine shops. Behind the shops the line goes through a hole in the wall to the staging area representing points along another 1000 miles of line. The layout has a mix of handlaid and flextrack, and the switches are controlled with Tortoise machines. The throttles are from NCE, and were easy to use and reliable. The only glitch in our operating session happened when some loose ballast got in a frog and derailed a freight.

After a coffee break, we headed on down the road to Peter Gilbertson's large HO layout - the Durham Subdivision of the CPR. It is in a dedicated basement room 34 by 26 feet. Peter says he designed the basement for the trains, and then built the house on top. He has been working on the layout for 8 years, and has designed it after the Kicking Horse Pass area. It is designed to be a point to point operation, with the mainline ascending via a helix to a second level yard. Currently the mainline runs in a 300 foot loop around the room, in a folded dogbone shape, with the main yard as the beginning and end of the run. A good deal of the spectacular Rocky Mountain scenery is finished, which makes operation a lot of fun. Peter operates modern diesel power, mostly Atlas/Kato, with NCE controllers, but isn't adverse to visiting steam power working a freight. Vancouver Island is home to many fine modellers. I hope you have enjoyed this little photographic tour as a sample of their efforts.



**Photo 5: CP C630, an Overland model, custom painted, climbs a grade in Kicking Horse Pass on Peter Gilbertson's HO layout.**



**Photo 6: The Master Feeds Mill is an accurate model of the namesake in Duncan B.C., and is a busy receiver of covered hoppers.**

**Photo 7: A scene on Wayne Paulsen's layout.**



## CHAPTER NEWS— continued

### Ontario Midwestern Chapter:

The spring meeting of the newly formed Ontario Midwestern Chapter was held at Participation Lodge in Holland Centre on April 10th.

The Grey Central Railway Club invited us to hold our meeting at the site of their layout. We gratefully accepted and enjoyed the opportunity to see the club's layout. Thanks to Randy O'Hara and members of the club for their hospitality. Some of our members had brought an engine or two and ran them on this excellent layout. Members brought quite a collection of photos which were of interest to members and guests as well as some models they were working on.

Next meeting will be held in Kincardine at the Knox Presbyterian Church, corner of Princess and Durham Streets at 2:00 p.m., Sunday, September 18th. We hope to have a couple of layouts to visit. For info, contact Randy (519) 371-1998, ont\_rail@yahoo.com or Art at (519)396-7694.

## LETTERS TO THE EDITOR

### continued

#### Durham's Rock Hard Water Putty

Several times a year in the various modelling magazines, authors recommend using Durham's Rock Hard Water Putty. It was mentioned at several clinics at ML2003 in Toronto. Yet it is very hard to find in Canada. Apparently in the US many hardware stores carry it. After some research I found the Canadian distributor:

York Enterprises, 75 Chambers Drive, Unit 9, Ajax, ON L1Z 1E1, 905-619-0115, fax 905-619-6583,  
[yorkflam@netcom.ca](mailto:yorkflam@netcom.ca)

Ron Collins was very gracious. He will sell from the company, which is just 1 block north of the 401. A four pound tin cost me \$14.99 plus taxes (total \$17.24). They do not have credit or debit card capability there so you must pay cash. Alternatively, one could special order it from a Building Materials retailer (you have to tell them to order it from York Enterprises because they won't recognize the name Durham's Water Putty).

#### **Dr. Glenn Bauberger, Peterborough**

#### Trains Canada

Are you aware that TrainsCanada made the decision last March (2004) not to publish any more books. This is a very worrying development, as it left both Richard Yaremko's Canadian Railcar Pictorial series and Fred Clark's Canadian Diesel Pictorial series without a publisher. I don't know what will happen to Richard's series, but I now understand that Fred may have found a publisher, although it will mean that subsequent volumes will be mainly monochrome. These books are an exceptional resource to modellers of Canadian railways. We also believe that they are good value for money, although, quite possibly it would seem somebody doesn't think so! Maybe there are too many people building imaginary railways, or making do with whatever comes out of boxes, rather than modelling the real thing. If subsequent books are published mainly in monochrome, it would be a disappointment, rather like diluting a good wine. If that's the only way, then so be it. If we are to promote Canadian modelling, these sort of books are exactly what is needed. We need to support Fred and his new publisher especially with our money.

**Ian Trivett, Ravenshead, UK**

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## COMING EVENTS

**St. Thomas, ON: Iron Horse Festival:** August 25-28 closes off 6 city blocks. Railway and modelling displays.

**Dearborn, MI: 25th Annual Narrow Gauge Convention:** August 31 to September 3.

**Brampton, ON, Brampton Model RR Show:** October 1, 2, sponsored by Orangeville Shortlines Model RR Club. Brampton Fairgrounds, 12942 Heartlake Rd, 8 kms north of Bovaird Drive. 10:30 pm to 4:30 pm. Adults \$5, Seniors \$4, Children 5-12 \$3, Under 5 free. Contact Dave Kennedy at [dektoy@sympatico.ca](mailto:dektoy@sympatico.ca) for more info.

**Regina, SK, Regina Model RR Show:** October 15, 16. Cochrane High School, 1069 14th Avenue East. 14,000 square ft of operating layouts, vendors, and railroadians. Contact yardmaster@hotmail.com for info

**Bowmanville, ON: 19th Annual Model Train Show:** sponsored by Soper Valley Model RR Assoc., Sat. Oct 15 /05 10:00 am to 4:30 pm, Sun. Oct 16 /05 - 10:00 am to 4:00 pm. Bowmanville High School, 49 Liberty St. N., Bowmanville, Ont. Adults \$5.00, Senior \$4.00, Family \$10:00

**Ottawa, ON: Railfair:** Algonquin College, Woodroffe Campus, Oct 15 & 16. Saturday 11 am to 5:30 pm, Sunday 10 am to 4:30 pm. Adults \$7, Seniors/Teens \$4, Children 5-12 \$1. Free parking. Featuring British, European, and North American vendors and layouts. A number of the layouts have been featured in the modelling press. For info, contact Tom Patterson at 613-831-1133.

**Montreal, PQ: CARM/CRHA 3rd National Convention—  
Victoria Day weekend 2006.**