



THE “CANADIAN”

a bi-monthly publication of the
“Canadian Association of Railway Modellers”
www.caorm.org

MAY JUNE 2005 ISSUE #9

COME TO WINNIPEG FOR GOLDEN RAILS 2005



LARRY MURPHY'S KIRKFIELD CRUSHED STONE

This will arrive in your mailbox just a couple of weeks before the start of the Golden Rails 2005 Convention. The people in Winnipeg have done a great job of putting together a wonderful program for you to enjoy. Dozens of informative clinics are planned, there are numerous railfan opportunities, including the CP Shops and in these pages we have shown you many of the fine layouts that will be open for you to visit. If you are able to be there this looks like one you won't want to miss.

BOARD HOLDS DISCUSSION WITH NMRA CANADA

The Board met with Representatives of NMRA Canada and the Niagara Frontier Region. At the meeting we discussed opportunities to work together to benefit Canadian modellers. A full report can be found on page 3.



RAILFANNING COBOURG



THE OBSERVATION PLATFORM

My musings this month are about the value of friends in the hobby. This month I have encountered numerous examples of it. The month started with three of us, myself, Craig McCulloch, and Colin Erwin pitching in to help Justin Parry put into place a schedule and card order operating system on his large N Scale Atlantic Superior Transportation. When you are staging in excess of 50, 30 car trains, this is not a minor undertaking, but many hands are making short work of the task.

During the last operating session at my house by our Rail Ops group I had encountered a serious short somewhere on the layout. My layout is Digitrax DCC with two power districts. After several days and many hours I was unable to localize the problem and ended up with most of the power leads disconnected. I discussed this with Craig, who is an electrical whiz, next thing you knew, Justin and Craig spent a day at my house and we tore out all of my wiring, moved my Digitrax boosters to a more central location, and started rewiring the layout. I now have 7 electrical blocks and a better ability to localize that short if it happens again. The rewiring has taken many hours work with 3 people. If I had to do it myself it would have taken weeks longer.

Towards the end of March I was on the weekly chat session hosted by CARM's Internet Moderator Richard Dilley and the conversation turned to my modeling of the Horseshoe

Curve. In the course of the conversation someone asked if the first generation diesel was still in the park at the top of the curve. I mentioned that it was but since I was modeling 1964 I needed to locate a K4 Pacific which was the original display locomotive. I mentioned that Trix had brought one out in the 70's and I had been looking at flea markets trying to find one. Within seconds I had offers from Craig Dunn and David King to supply the locomotive. I sent Craig an appreciative note but since Dave is a fellow club member who lives near me I took him up on his offer. When I enquired what he wanted for it he jokingly said "just call it the Dave King Memorial K4". I responded "no problem, but I not sure anyone will be able to read the plaque in N scale".

By the time you read this I will have hosted a club operating session at my layout and there will be a sign on the fascia.

DAVE KING MEMORIAL K4 PARK

There once was a modeler named Dave
Who lived in these hills in a cave
He modeled in N
Till he developed the On30 yen
It got him so bad, he couldn't be saved
So when you look at the park, remember Dave.

Friends, words can not do justice to the value they add to your enjoyment of life and the hobby.

John Johnston: Editor

PASSWORD CHANGE

MEMBERS AREA: EFFECTIVE MAY 5

USERNAME: coupler

PASSWORD: knuckle

JOIN THE CARM MODELLERS LIST ON YAHOO

Do you have the Internet? Are you interested in a dialogue with other modellers across the country? Then go to www.caorm.org and follow the links to sign up for the CARM Modellers List located on Yahoo.com. In addition to the e-mail list the site hosts monthly chat sessions.

COVER PHOTOS

Upper Photo: Peter Moffett: Scratchbuilt crusher and support buildings on Larry Murphy's, Kirkfield Crushed Stone. Stone comes from the quarry on the narrow gauge, is cabled up the ramp to dump into the crusher. The cars are then released down the track to the right to return to the quarry.

Lower Photo: Ted Rafuse: VIA Rails F40PH-2 painted to commemorate the 50th anniversary of the CBC, stops at Cobourg, Ontario.

BACK ISSUES AVAILABLE

Back issues of the Canadian are available and may be purchased for the cost of production and mailing. The costs are:

Canadian Members: 1 Issue—\$2.75 Each additional issue \$2.00

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Costs are in Canadian dollars, the increased costs for U.S. and European members reflect the increased costs of mailing.

TO ORDER BACK ISSUES CONTACT THE EDITOR

Material for the Canadian should be sent to:

Editor: John Johnston, 41 Glenview Place, Hamilton, Ontario, L9C 6H9 or by e-mail at editor@caorm.org

REPORT FROM THE CHAIR

Carm and NMRA

On March 8th, the Board of Directors met with Clark Kooning who is spearheading the move towards NMRA Canada, as well as Chuck Faist and Walter Reid, who were representing the Niagara Frontier Region of the NMRA.

There was a wide ranging discussion of the current circumstances. It was decided that co-operation between CARM and NMRA in the delivery of some local services would benefit both organizations. It was also concluded that at the current time this would have to take place between CARM and the individual NMRA Regions. This may change in the future dependent upon the development of NMRA Canada and its role. The following matters were agreed upon:

- 1: CARM and NMRA Canada are totally independent organizations
- 2: CARM may contact NMRA Regions in Canada, such as the Niagara Frontier Region, to determine if we can work co-operatively in scheduling joint activities such as meets, train show activities, etc. NMRA Canada will support these activities.

It was noted that CARM was commit-

ted to having discussions with its members to ensure that there was support for this co-operation. CARM plans to hold these discussions at its National Convention in Winnipeg in May. We believe that co-operating locally will ensure that modellers have the greatest level of access to modeling activities at the lowest cost.

Chapters:

If you are thinking about setting up a Chapter, take a read of the report by Ed Warren from British Columbia, on the Chapter News page. Minimal structure, no politics, and getting together two or three times a year to share good fellowship. It really is that simple. If you have been thinking about a Chapter but haven't yet decided to take the step, this is very informative stuff.

Winnipeg Annual Meeting:

If you are going to be in Winnipeg, then come out Sunday morning to the Annual Meeting. Like last year I will be available for general discussions about CARM and I look forward to receiving your input and suggestions about how we can make this Association better.

John Johnston: Chair

FINANCIAL STATEMENT 2004

Revenue:

Membership:	\$17,884.68
Apparel:	\$ 2,788.19
Calendar:	\$ 580.00
Newsletter:	\$ 51.25
Rebates:	\$ 170.00
Bank Interest:	\$ 5.47
Convention:	\$ 2,199.00

TOTAL REVENUE: \$23,678.59

Expenses:

Newsletter:	\$ 8,148.05
Web/PR:	\$ 2,420.86
Membership:	\$ 644.57
Gen. Admin.	\$ 2,196.87
Capital:	\$ 2,466.32
Apparel:	\$ 4,496.09
Chapters:	\$ 177.50
Convention:	\$ 747.71
Calendars:	\$ 812.89
Reserve:	\$ 1,567.73

TOTAL EXPENSES: \$23,678.59

NOTES:

- 1: Directors and Officers received no payment or honorarium for their work on behalf of the Association.
- 2: Monies paid in 2004, but which are applicable to future years dues are not shown on this statement. Those amounts are \$13,395.06 carried into 2005, \$3,977.85 carried into 2006, and \$1,220.58 carried into 2007. This accounting method ensures that money paid for future years member services are not taken into revenue until the year in which they will be disbursed.
- 3: The difference between revenue and expenditure for Apparel is offset by Apparel currently held in inventory pending sale.

Peter Moffett: Secretary/Treasurer

2006 CARM CALENDAR
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KIRKFIELD CRUSHED STONE

Larry Murphy's Modular Layout in HO Scale: Article and Photos by Peter Moffett, CRC

Kirkfield Crushed Stone Company existed near Kirkfield, Ontario on the CNR's Cobocconk Subdivision (ex Toronto & Nipissing). Kirkfield is located just east of Lake Simcoe in Ontario. Actual distance is 100km north and 30km east of Toronto. The prototype existed alongside the Trent Severn Waterway about ½ mile upstream of the Kirkfield Lift Lock. The quarry existed to provide crushed stone for a variety of purposes, one of which was ballast for the railway. The complex consisted of a large quarry and a crushing plant. There existed a narrow gauge railway that delivered stone from the quarry to the crusher. The crushed stone was delivered to customers from the crusher by the standard gauge CNR. The whole complex lends itself beautifully to a modular railway which is exactly what Larry Murphy has built in HO scale.

Larry's concept was to recreate a piece of history from Victoria County and the flavour of an Ontario narrow gauge industrial railway circa 1954. The inspiration for the layout was an article by Niall Mackay ("The Kirkfield Crushed Stone Company") that appeared in the Jan/Feb 1988 Narrow Gauge and Shortline Gazette. Larry also had personal knowledge of the prototype and photographs collected from Robert Sandusky, R.L. Kennedy and Ron Stewart.

The layout consists of a collection of standard modules. There are three 2'x4' modules, one 6'x2' module and a standard NTRAK 4' corner module. The modules are constructed from 1"x4", 1"x3" and 1"x2" dimensional white pine with a Masonite fascia and profile board. The legs are metal and with adjustable feet so the layout can be levelled easily. Height of the layout is 44" above the floor.

The sub roadbed for the trackwork uses a door skin over a Styrofoam base set into the pine frame. This creates a strong but light module which is a definite asset when moving the modules from house to car and back again for display at train shows. Sheet cork is then glued to the door skin for the roadbed.



Photo 1: This is the quarry section of the layout built on an NTRAK corner module. N scale flextrack was used and the engine is a scratchbuilt body on an N scale chassis. The dump cars are N scale from Roco. The shovel was scratchbuilt.

Photo 2: The prototype Kirkfield Crushed Stone Company as it was in 1961. Dump cars are pulled up the ramp into the crusher by a cable between the rails. Empty cars are returned through the spring switch to the receiving track on the right where they wait to be returned for loading in the quarry.





Photo 3: The standard gauge CNR is spotting a string of empties under the tipple ready for a new load of crushed stone. This plant connects to the Coboconk Subdivision of the CNR near Lake Simcoe about 80 miles north east of Toronto.

The narrow gauge portion of the layout delivers stone to the crusher from the quarry and Larry used Atlas N scale code 80 flextrack. The standard gauge track which represents the CNR mainline and sidings to the plant was laid with Atlas HO standard gauge code 100 flextrack. Switches are all Peco. The minimum radius on the narrow gauge section is 18" with wider radius curves on the standard gauge.

The minimum radius on the narrow gauge section is 18" with wider radius curves on the standard gauge. The only grade is on the narrow gauge and the maximum is 5% with a total drop of about 7" down into the quarry. Ballast is Woodland Scenics with some sifted kitty litter (I hope the cat doesn't find it, ed.) added to the mix and secured in place with diluted white glue.

The scenery was built up from Styrofoam and covered with drywall compound and painted with latex. This base was covered with layers of Woodland Scenics ground foam. The rock faces were coloured with water based craft paints. The scenery was finished off with trees from Woodland Scenics and also some handmade trees using natural weeds as armatures.

Meet Larry Murphy



Larry is retired but works part time for Statistics Canada. He has been married to Elaine for 42 years and has two sons and two grand children. Larry builds dioramas and layouts by contract for museums, corporate and private clients and also does some custom painting. He likes the decade of the 50s and he customizes locomotives, cars, etc. to represent this era. His home layout is HO and freelance. His other interests are music and he has a background in drum corps with emphasis on percussion and brass arrangements. He plays French Horn and Trumpet.

The narrow gauge engine started life as an N scale Life Like GP38. Larry used this chassis and scratchbuilt a styrene cab. There are also steam engines on the layout consisting of two Roco HOe 0-6-0 saddle tankers, and one Mini Trains 0-4-0 tank engine. The standard gauge engines consist of one modified Bachman Spectrum 70 ton diesel, one Tenshodo 0-4-0 Docksider, a modified Bachman 2-8-0 and various Athearn CN diesels.

Rolling stock consists of narrow gauge N Scale Roco "V" dump cars. Standard gauge cars are from a variety of manufacturers including Accurail and Athearn. The shovel in the quarry was scratchbuilt.

The structures on the layout (main crusher, workshops, hydro substation, water tanks, coal bunker, company store, company houses) are all scratchbuilt since there was nothing available commercially that came close.




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PHOTOS FROM TRACTION FAN—OLIVER McKEE



Photo 1: Suydam tower car



Photo 2: Suydam wood style Interurban

RAILFANNING COBOURG, ONTARIO

Article and Photos by Ted Rafuse

As part of my modelling interest, I occasionally engage in rail fanning or train watching. As a novice, I am never certain whether I am rail fanning or train watching when I am trackside in my hometown of Cobourg, Ontario. Rail fanning suggests to me purposely “going somewhere” to watch trains while train watching implies that I go to my hometown VIA station to observe passenger trains operating between Toronto and Montreal/Ottawa and CNR and CPR freight trains operating between Toronto and Montreal. Whichever term applies, that’s what I can do with ease!

This pastime began as a direct result of baby-sitting my then two-year old grand son, Shay. I am an HO modeller, he plays with Thomas the Tank Engine. One day at that tender age, I decided to take him to the Cobourg train station to watch trains go by. I don’t recall now whether it was late Spring or early Fall, but we first stopped at Tim Horton’s for takeout apple juice and chili before we headed out on our ‘trek.’ I then drove to the station and parked at the west end of the lot. We tailgated between 11:45 and 13:15 during which time we saw several VIA, CN and CP trains. He was hooked and I was pleased with our reactions to this new activity. In the intervening three years we have had numerous male bonding tail-gate lunches at the station parking lot.

Cobourg, Ontario, is situated about one hour’s drive on highway 401 east of Metro Toronto. To get to the station, take the Highway 45 South Exit onto Division Street. Continue, or not, past the Tim Horton’s and other fast food services, and follow the VIA depot signs. As soon as you pass underneath the first railway bridge, turn right (west). If you pass under the second railway bridge you have gone too far south, but you can easily turn around. Once on the street between the two railway underpasses, climb the hill,

pass the station, and park in the far west end of the parking lot.

Once on site, you are between the mainline CN double track to the north and the mainline single CP track to the south. The two system tracks are approximately 30m/100ft apart at this point and both are on a gentle arc. To the east is the GTR/CNR/VIA stone and brick station, built in 1911, and situated on the south side of the tracks. To the south, and on the south side of the CP track, was once a CPR station unfortunately demolished more than a quarter of a century ago. To the west, a multiple signal aspect straddles several CN tracks. When lit, these block signals are useful in foretelling the imminent arrival of a train. There is a single aspect on the CP track, but its visibility depends upon where you might be standing. You must walk west off the parking lot to see this signal.

Photo 1: Of late, CP trains are often led by “foreign” power, but capturing 2 Soo units in different paint schemes, is not common.



The best indicator of CP traffic, which gives ample warning, is the contact that operates the flashing lights and bell at the road which crosses the CP tracks near the middle of the parking lot. Unfortunately there are no train whistles in Cobourg to alert one to the arrival of trains.

A visit to the train station is in order as part of the activity while you are here. Several years ago VIA Rail extensively renovated the station, but unfortunately replaced the older wooden bench seats with chrome and vinyl seats. The waiting room has vending machines, and importantly, provides rest room facilities. The Station Bistro, operated by a local entrepreneur, provides good food at less than GTA prices and provides a view of both mainline tracks while dining.

Trains are any time at this location. For the past decade a group of rail/model friends have participated in an annual 24 hour train watching at this site. During that ten year period, 65 trains are observed on average between noon on Thursday and noon on Friday. Most frequently the vigil occurs the first week following the Canada Day weekend. Their records indicate that between 0130 and 0430 is the only time that may have little train traffic but that is no guarantee on any given day.

Weekdays between 0430 to 0900 witnesses several VIA trains as well as a number of CN and CP trains in either direction. The CN "Belleville Turn" usually appears sometime between 1000 and 1200 but sometimes can arrive after noon. When it arrives is dependent upon the number of cars to be shunted at any given location. While in Cobourg, it generally parks its train to the east of the station while it shunts the industrial park from the switch at the east end of the overpass. Occasionally it picks up or delivers cars to the interchange track farther east. To the north and west of the station, is another industrial track which the shunter plies, but this movement is often blocked from view by parked rail cars in the yard.

However this problem is easily solved by driving north of the station to the first street, aptly named Station Street. (Station Street is so named, as at one time, to the east side was the Canadian Northern Railway station.) Turn left (west) and drive to the clearing at the end of the road.

Photo 4: East bound Via about to depart for Montreal.



Photo 2: CN westbound with 5743, 5279 and 5358.



Photo 3: Having switched the industries at the west end of town, 4108 and 4138 are returning to the yard on the former roadbed of the CNoR.





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There the track splits, one track going further north, the other to the west. The westbound track sees most of the industrial movements. This section of roadbed was constructed by the Canadian Northern Railway in 1911 and is the only part of the CNoR still in use locally. The section of roadbed to the north still in use was built originally by the Cobourg & Peterborough Railway in 1853. Both railway properties are have been part of the CNR since the latter was formed. (The current CN mainline was built by the GTR in 1853 and the current CP mainline was opened in mid 1914.

Cylindrical and slab side covered hoppers and flat cars of wood are the most frequently observed freight cars in the yard, with less frequent appearances of box cars and gondolas. Frequently there is a contingent of work cars in the yard near the yard office buildings on the north side of the yard. **DO NOT** trespass on any of the tracks for obvious reasons. Another reason not to do so is that the area is patrolled by CN and Cobourg police.

As mentioned earlier, around noon is best for train watching. Between 1430 and 1730 is another time of peak activity. Several VIA trains, and CN and CP movements, in both directions, normally take place. (On rare occasions, track work may restrict movements at any time.) At any time, with good luck, two trains can occasionally be photographed simultaneously, and rarely, three trains can be photographed in a single frame. For those interested in night photography and/or video taping, the station platform is illuminated. A word of warning however, the station is closed at night, and no facilities are available overnight!

On most Friday afternoons a contingent of Cobourg Railfans appear. No formal admission is required to this group other than an interest in trains. Conversation is lively, continuous and informative. This all ceases upon the arrival of a train, or trains, when the photographers and videographers scramble into action. There are more than 100 nominal members of the group, with some twenty active members. The Cobourg Railfans have a web site established by the group's mentor, Milne Hall. Their web address is <http://groups.msn.com/cobourgtrainfans> There you can find information regarding rail news, scanner frequencies, schedules,



Photo 5: CP 9133 singly handles an Eastbound container train.

pictures, OS for all three railways, train shows and much more. Milne frequently reports sightings from this location for the periodical [Branchline](#).

Scanners generally receive information from two detectors east of Cobourg. The CN Kingston sub provides name, mileage and speed of CNR and VIA trains. The CP Belleville sub provides name, temperature and number of axles. Both provide an indication of east and westbound trains. With a good antenna, detector information can be picked up from sites west of Cobourg to indicate approaching east and westbound trains. Additionally, scanners will at times receive communications between radio traffic control (rtc/dispatcher) and, on CP, train crews communicate mile boards in the area.

The last Friday in September of 2004 was a wonderful summer day, and I decided to do some train observation as part of the research for this article. Between 1415 and 1530 ten trains passed: three CP, including the Agincourt turn which pulled some cars from the interchange, three CN trains, two westbound and one east bound, and four VIA trains. The accompanying images lend a sense of the action that occurred. The author is still on a steep learning curve in the use of his digital camera, so the accompanying photographs are only indicative of the possibilities of image recording here.

Cobourg has special appeal as a destination during the warmer weather. A spouse and children can enjoy an outing at Victoria Beach, perhaps the finest beach on the north shore of Lake Ontario. You can be dropped off at the station and the family can take the vehicle further south on Division Street right to the lake. To the left is the beach, to the right is the marina. With an easy site to reach, and a safe environment, Cobourg train station is a great rail fanning / train watching site. Try it out!



Photo 6: Westbound VIA about to depart for Toronto.

CANADIAN AUTHORS

STEAM TO THE NIAGARA FRONTIER

Article by Ian Wilson - Photos from his book STEAM TO THE NIAGARA FRONTIER

I was born in 1960, and raised in Barrie, Ontario. Over my childhood years, I cultivated an interest in steam locomotives, particularly those which operated in Southern Ontario. Credit for sparking this interest goes to my parents, especially my father (Mac Wilson). He grew up in Port Credit, Ontario, near the busy Oakville Subdivision. Through his childhood and early adulthood, he witnessed a non-stop parade of steam power from the CNR, CPR, TH&B, NYC and other roads (e.g. leased units in the late 1940s).

By the late 1950s and through the 1960s, family outings often revolved around "chasing" steam excursion engines such as 6167 and 6218, and visiting and photographing railway stations which were slated for demolition. As a child, I pored endlessly through Dad's back issues of the UCRS Newsletter, issues of Railroad Magazine from the 1940s, and various books such as "Canadian Steam!" and "Canadian National Steam Power". Dad's collection of model railway magazines (MR, RMC, Model Trains and various books) were also read and re-read. As such, I developed a parallel interest in railway modeling, which progressed from a Lionel layout with my brother to scratchbuilding structures and rolling stock in HO scale by my early teen years.

Through highschool and university (BSc. Civil Engineering, Queen's University, 1984), I remained active in the hobby of railway modeling and photographing steam era railway equipment (especially steam locomotives). As early as the mid-1980s, I had a desire to re-create the steam railway era at my hometown Allandale (Barrie). I knew that a wealth of railway history connected with that division point was still within reach through surviving steam men, but disappearing rapidly. At the same time, I lamented the state of the railway modeling hobby in Ontario. With

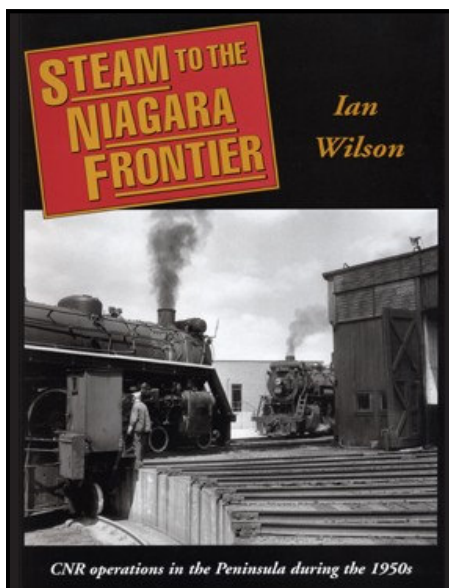
so much in the way of local prototypes, I longed to see examples based upon that scene, but was largely disappointed.

Through the early to mid-1990s, my interest in prototype railway modeling became focused on the CNR in Southern Ontario during the 1950s. This was a throw-back to my childhood visits to railway branchlines in the province, when there was enough remaining structures and facilities to suggest a more active role in the years prior to my birth. The desire to know and understand the railway operations as they existed in the final years of steam--just before my time--became the motivating force for me in the hobby. A steady stream of books in the 1980s and early 1990s from Boston Mills Press fuelled the desire further.

By 1996 I was married, but flailing without direction in my career. For several years I had been self-employed, but having moved from Toronto to Orillia I had removed myself from the market I previously tapped. Furthermore, very little in the world interested me aside from the study of steam railway operations in Ontario. My new wife Mary-Jo,

knowing me all too well, urged me to take the pressure off myself. As I was providing little in terms of monetary worth to the two of us anyway, she suggested that I take a full year to pursue any path I wished, without worrying about whether it generated income or not.

Thus, I decided to write an article for CN Lines based upon some paper records in my possession from the former Palmerston, Ontario CNR l.c.l. freight transfer shed. In so doing, I was recalling an encounter with Stafford Swain in 1993, when he urged me to write an article about any CN-related project I might pursue.




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When I directed an inquiry to Stafford, he replied instantly in the affirmative. With the help of Don Grove and Peter Bowers, I spent a month writing the article, and editing it at the home of Al Lill during my honeymoon in July 1996. A few months later, the article appeared in CN Lines, and was well received. Shortly thereafter, the editors of Railroad Model Craftsman and RailModel Journal advised that they would purchase any articles I decided to write. In the early winter of 1997, Mary-Jo and I went out for a celebratory dinner.

A stream of articles in quick succession followed the Palmerston piece into CN Lines. With each project, I was visiting places from the past, which I wished I could have seen from my own eyes: Owen Sound, Port Credit, Penetang. In fact, from the start I had adopted a writing style of describing events in the present tense. This is a genre in the field known as "creative non-fiction", and I had honed my skills in this area for a number of years in speech writing.

By early 1997, I was aware of the fact that the railway story of my hometown of Allandale (Barrie) was untold. Therefore, I decided to write a series of articles dealing with each branchline radiating from Allandale, beginning with the Penetang Subdivision. Peter Bowers pointed out to me that a book on Allandale was desperately needed, and furthermore he would be of no assistance to me unless I promised to write such a book! I agreed to his ransom demand.

Throughout 1997 and the first half of 1998, I worked feverishly putting together "Steam at Allandale". By now, my research and writing had evolved into a regular routine, so

preparing the book was no different than producing a series of articles. Dave Shaw was enlisted for making 8x10 glossy prints from negatives supplied, Ian Cranstone was coerced into designing the book, and 357 people believed in the project enough to lay their money on the line by prepaying for an unseen book from a fledgling author.

"Steam at Allandale" was released in Allandale on August 31, 1998, and by Christmas it had sold more than 2000 copies. Meanwhile, having carved a new career for myself, I dove into the next volume, entitled "To Stratford Under Steam". To the present day, I have followed an identical format with the books, releasing a new one every 18 months. With the exception of Ian Cranstone assisting with design of the first three volumes, I have produced these singlehandedly. Following the Allandale and Stratford editions, subsequent volumes have covered CNR operations in and around Palmerston, London and the Niagara Peninsula. Future books will deal with the Northern Ontario District in its entirety (up next), the Belleville Division, Hamilton, and possibly Toronto and the Montreal District.

Over the past few years, Mary-Jo and I have been blessed with the arrival of two sons, Spencer and Duncan, now aged 4 and 2 (as of January 2005). We continue to make our home in Orillia, Ontario. Indications are that the two young fellows believe trains to be a pursuit worthy of interest, models and the prototype, and they both have a clear preference for steam over diesel!



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To my knowledge, I am the only person in Canada to endeavour to support a family, with no government funding, on the fruits of my labours in the field of writing and publishing historical railway material. I do not recommend this to anyone else, for I believe that such a career was not of my choosing as much as it was thrust on me. Since the early days of my writing, I have sensed that this is the purpose in this life to which I have been called, and I will not quit until my work is finished. Even if someone handed my \$10 million tomorrow, I would continue with the task of telling the story of the postwar steam era on the CNR in Ontario.



For more information on publications, by Ian Wilson look at the website www.canadianbranchline.com



SALE 2005 CARM CALENDARS



Originally \$15, these wonderful calendars with both prototype and model photographs can be purchased for \$10, while supplies last.

Calendars can be ordered from the secretary (secretary@caorm.org) and payment may be made by cheque (payable to CARM) and sent to: CARM Secretary, 7 Jolie Court, St. Catharines, ON L2M 6V5.

Payments may also be made using PayPal (www.paypal.com) with payment being made to treasurer@caorm.org.

CARTOON BY PAUL ULLRICH

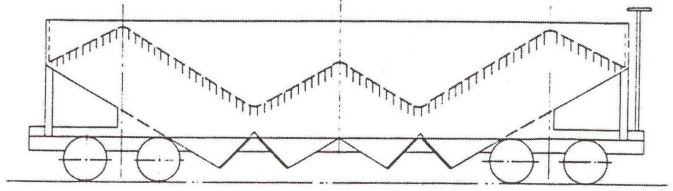
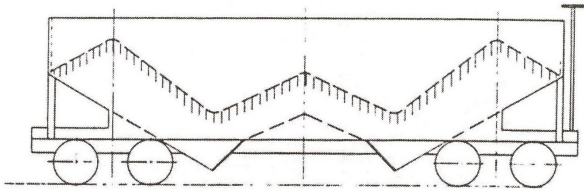


STEEL MILL SHIPMENTS - PART 2

By Ron Tuff and Ray Hoadley

This issue we take a look at prototype drawings of granular hopper loads and various models demonstrating the loads. What is important to note is that a not every hopper is filled to the top rails with material. Heavier materials such as iron ore are loaded over the axles. With some of the lighter loads such as coke, hopper cars were fitted with side extensions to enable a greater volume of material to be carried. The types of loads which would find their way to your steel mill yard would include coal, iron ore, coke, limestone, and regular quarry stone.

LOADING IRON ORE, LIMESTONE, AND SIMILAR MATERIAL IN HOPPER AND GONDOLA CARS

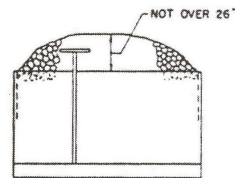
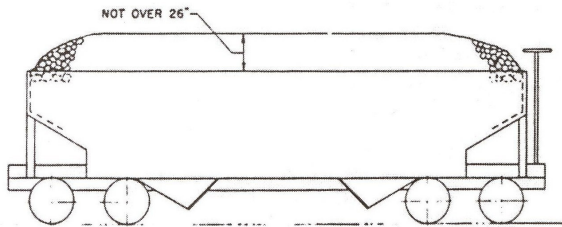


Ray Hoadley's G Scale model of TH&B 1214 with a load of iron ore.



Ray Hoadley's HO Scale model of CPR 357300 loaded with iron ore

LOADING COAL, STONE, AND COKE IN HOPPER AND GONDOLA CARS



Ray Hoadleys G Scale model of CPR 358125 with 3 ft extensions for carrying coke.

Ron Tuff's HO Scale model of Bay City 69122 carrying a load of stone



IMPROVING HOPPER LOADS - ADDING LIFT OUTS

By Fred Headon MMR CRC

The hopper car loads that are included with many hopper models from the factory have a number of faults. The colour is too uniform, the edges of the load are often visible and they often sit too high in the car with the edges of the load overflowing the top edge of the car. The solutions to this problem are simple and involve a little flat black paint and some filing.

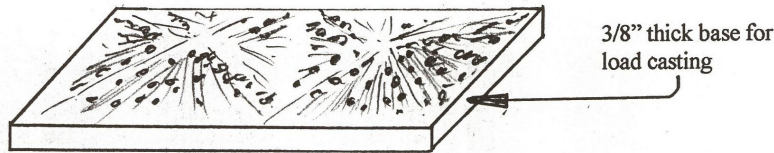


Figure 1: This shows the typical generic factory load. The square edges and overall depth of the casting cause the load to sit too high in the car.

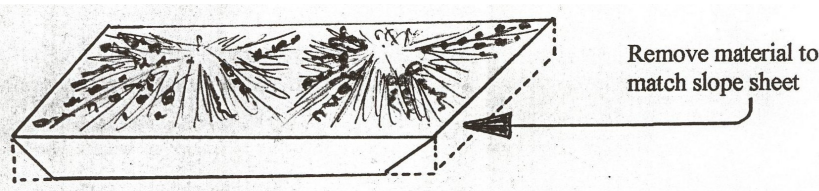
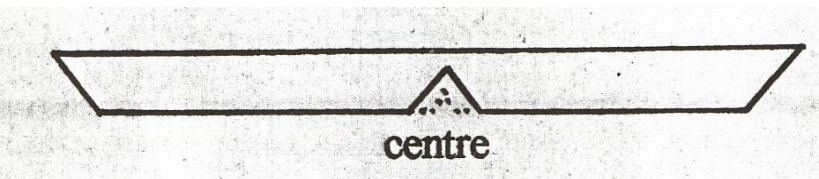
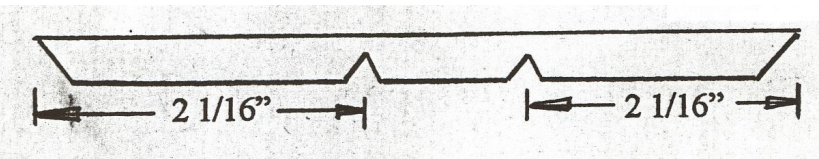


Figure 2: Touchup the loads with some flat black paint. At each end, file away material to the angle of the hopper car's slope sheet. File away a little bit from each side of the load to make it easier to put in or lift out.



Figures 3&4: To assist in "snuggling" the load into the car, file a notch, or expand the notch cast into the load so it clears the interior bracing of the hopper car. This will be at the centre for a two bay car as show in Figure 3 and will be about 2 1/16 inches in from each end for a three bay car as shown in Figure 4. The notch should be slightly deeper for a two bay car. A little experimentation and fitting will help you find the right depth.



EASY LOADING AND UNLOADING OF OPEN TOP HOPPER CARS

Figure 1 Below: Draw diagonals from corner to corner. At the centre drill a #63 hole that will clear a straight pin.

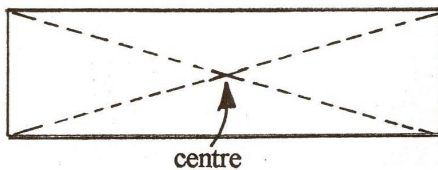


Figure 2 Right: Insert a straight pin the hole. Check for easy up and down movement. Bend the bottom of the pin at a 90 degree angle, leaving 3/8 inches of the pin below the load. Paint the head of the pin to match the load.

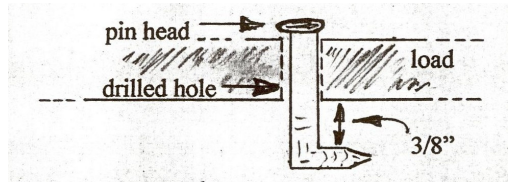


Figure 4 Below: It is easy to place the load into a car. Lift the load with the pin head. The pin will drop into the load and be out of sight when the load rests in the car.

Figure 3 Right: When sitting on a flat surface the stored loads are ready for easy pickup, due to their extended pin.

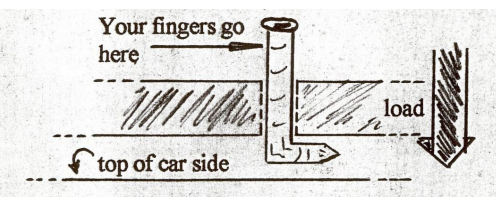
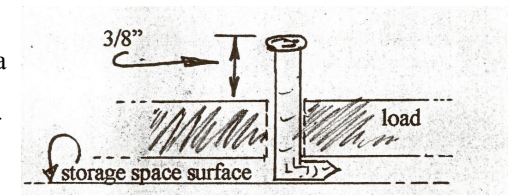
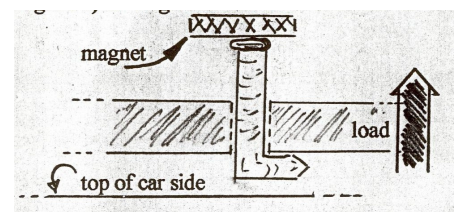


Figure 5 Right: A magnet makes the task of lifting out loads easy. The magnet will pull the head of the pin up about 3/8 inches. You can then grab the pin and lift the load clear of the car.



CHAPTER NEWS

British Columbia Chapter:

The well established Nanaimo train show is put on by two local modellers, Rick Lord and Ken Rutherford. They were happy to have a CARM table, saying that they were happy to support any organization that supported the hobby. The billboard display arrived at just the right time, and most of the membership applications and many of the business cards were picked up by the general public. There was a lot of interest by the 1500 attendees in CARM, and I was pleasantly surprised as it was our first formal promotion effort in BC. I also did a clinic on dry transfer lettering at the table.

We will be attending the Victoria train show on September 24, 2005, and the Burnaby show in November. It appears that many MR's are looking for an organization like CARM to meet their needs.

I arranged a lunch table for CARM members at a restaurant next to the arena. We held a first meeting to see if members were interested in forming a local chapter. Six people were there, and two others were tied up at the show and unable to attend. Andy Barber seemed to reflect the majority opinion, when he stated that most of his MR social needs were met by his operating group, so there was no need for another club with regular meetings. But there was a need to have contact with a larger geographic area, and everyone was keen to have a method for that. Members are scattered over a 250 km distance, from Campbell River to Victoria. We decided that we would have lunch as a CARM group at shows, take a break from the noise and bustle of the arena, and have a chance to visit in a congenial setting. We also decided that it would be nice to visit each other's layouts as a group activity on an irregular basis. We decided that there was no need for any formal structure. We will be gathering in Qualicum on May 7th to drive down to Andy Barber's, just north of Victoria, to visit his layout for an operating session, We will have lunch on the way. It should be a nice outing.

I think every area will have to find out how it will want to have their chapter set up, as member needs and geography

dictate. Dick is going to see what he can do on the Lower Mainland, as there is a good concentration of members. The B.C. interior is a much more difficult problem, as the members are scattered very widely. Judging from the response at lunch yesterday, CARM members are very happy to have a local chapter, so we will have to keep working at it provincially. People were quite interested in the calendars you sent. Look forward to seeing you in Winnipeg.

Ed Warren

Toronto Chapter:

The Toronto Chapter has planned for the next couple of months.

- May 23—breakfast and slide show at Convention
- June 27— visit to Delaware and Rutland Model RR

Golden Horseshoe Chapter:

As we go to print the Golden Horseshoe Chapter is holding its first meeting at the home of Andy Panko where Andy's layout and clinics will be the order of the day. We should have a report for the next issue of the Canadian.

FORMATION MEETING

Ontario Midwestern Chapter:

I would like to announce a meeting to discuss the formation of the Ontario Midwestern Chapter. We are meeting at the Country Caboose in Elmwood Ontario on Sept 19th at 2:00pm. If you live in Grey and Bruce Counties or feel you live close to these counties and would like to participate please feel free to come. You will need to bring your own lawn chair. The schedule of events hasn't been finalized.

The Country Caboose is a unique Rail only Hobby Shop and may have what you are looking for. So please come out rain or shine and we'll all have a good time. For more information you can email me at cnmodeler@yahoo.ca and for those of you without email 519-371-1998 ask for Randy. Thanks very much and I look forward to meeting everyone

Randy O'Hara

COMING EVENTS

Lindsay, ON: Model Transportation Expo/Trains, Boats, Planes, Automobiles-April 30 & May 1
Victoria Park Armories, 210 Kent St.W. Sat. 10 a.m to 5 p.m. Sun. 10 a.m. to 4:30 p.m. \$4
Sponsored by Lindsay & District Model Railroaders and Lindsay Model Shipwrights.

Guelph, ON: Guelph Model RR Society Open House-May 1. 50 Crimea St. West of Edinburgh Rd N near the GEXR tracks. 10 a.m. to 5 p.m. \$4 519-856-4983 or 905-878-5009.

Innerkip, ON: Hank Duivenbode S Scale Open House-April 30 and May 1. RR#2 Innerkip. 11 a.m. to 4 p.m. No fee. Hank Duivenbode 519-469-3695

St. Thomas, ON: Railway Nostalgia Weekend-May 7 & 8, at the Elgin County Railway Museum located in the old Michigan Central Shops, 225 Wellington St. Vendors, displays, rolling stock, model railroads, train rides, and a food concession. For info contact 519-637-6284 or e-mail promotions@ecrm5700.org

Aldershot, ON: Burlington Model RR Club Open House - May 28, in conjunction with the Aldershot Garden Festival. Layouts will be on display from 10:00 a.m. to 4:00 p.m., weather permitting G Scale layout will be operating. HO and N Scale layouts inside the building. Located in Hidden Valley Park. Admission by donation. Info from Jim Torrance at 905-335-1177 or wtorrance1@cogeco.ca

Midland, ON: 17th Annual Model Railroad Show- May 28 & 29, sponsored by the Midland District Railroad Club at the North Simcoe Sports and Recreation Centre on King St. Admission \$4, Saturday 10a.m. to 5p.m., Sunday 10a.m to 4p.m. Contact Ron Edwards 705-526-2697

St. Thomas, ON: A Day out with Thomas-at the Elgin County Railway Museum located in the old Michigan Central Shops, 225 Wellington St. The favourite railway engine of thousands of children will be featured at the Museum. Thomas the Tank Engine will be giving rides to children and those who wish they were children. All ages \$18 plus taxes, children under 1 year free. Thomas will be at the Museum July 22, 23, 24, 29, 30, 31.

Winnipeg, MB: CARM 2nd Annual Convention:Clinics, Tours, Layout Visits - May 20,21,22