



# THE "CANADIAN"

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ASSOCIATION OF RAILWAY MODELLERS  
FOUNDED IN 1958

MAY JUNE 2006 ISSUE #15

## MONTREAL CONVENTION JUST WEEKS AWAY

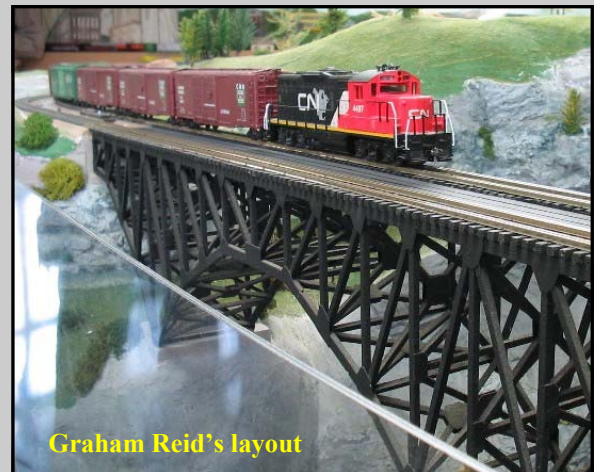
It is only a few weeks until our next National Convention to be held in Montreal, Quebec on the Victoria Day Weekend May 19th to May 21st. This will be our 3rd convention, following St. Catharines, Ontario, and Winnipeg, Manitoba. These conventions, and next years, in Victoria, British Columbia, have established the presence of CARM across this great country. If you haven't attended one, now is the time to do it. Sign up to attend Montreal, there is still time, and join your fellow modellers for a weekend of good fun and modelling.



Jim Kemp's TH&B 4-4-0



Ron Scott's freelanced TH&B layout



Graham Reid's layout

a bi-monthly publication of the "Canadian Association of Railway Modellers"



## THE OBSERVATION PLATFORM

First, an update on the reconstruction of the layout. I found it hard to believe the extent of construction materials that go into even a medium sized layout. I arranged for a dumpster and to my surprise managed to fill it with the layout and the remains of the ceiling in my basement which I also ripped out. This new layout will fill a slightly larger space and visitors will no longer have to go through my storage room to gain access, which meant that walls had to be moved and with them wiring. As I write this, all of these changes have been accomplished and the only major construction task left is putting in the new drop ceiling which should be finished later this week. Benchwork construction will commence shortly thereafter and hopefully by the next issue I can advise you of substantial progress in actually building the layout.

Having made the decision to base the new track-plan on a more prototypical representation of the area around the Horseshoe Curve, I began the process of researching what the area looked like in 1964. I have numerous books chronicling the Pennsy and these proved an invaluable resource but much more was needed. I began to scour the Internet and was pleasantly surprised by the wealth of detailed information that is available.

I was able to locate actual Pennsylvania Railroad track plans and interlocking diagrams for the entire mainline between Harrisburg and Pittsburgh including the area between Altoona and Cresson which I model. Numerous pho-

tographs of structures, towers, and track details can be found. Many of you are probably familiar with George Elwoods fantastic website containing thousands of railroad photos, [www.rr-fallenflags.org](http://www.rr-fallenflags.org) This collection covers all of the railroads in North America and his Canadian collection is extensive. If you haven't visited this website, give it a look.

I have found one minor problem. A centerpiece of the layout will be the station at Altoona. I have a number of photographs which show the station, but all are taken from a distance. I will have to scratchbuild this station and some close up photos would be helpful. I am continuing my research and am hopeful of finding some detailed drawings, however, if any of you my age made railfan trips to the Altoona area in the 60's and had photos of the station you were willing to share I would be eternally grateful.

With a new grandson, my railroad interests have also taken another turn. He just turned one and he has found three people who interest him besides Mom, Dad, Grandma, and Grandpa. Barney, Dora the Explorer, and Thomas. It is really neat to see his face light up whenever we open a Thomas book, watch a Thomas video, or go into Toys-R-Us and he heads straight for the Thomas section. Ron Bareham and his colleagues at the St. Thomas Railway Museum are holding Thomas Days on just about every weekend in June this year and I have purchased tickets for the train ride. It will be fun to see the excitement and wonder in the little guys' eyes. If you have a little one, tickets can be obtained through Ticketmaster. I'm sure that this is a good fundraiser for the Museum.

Changing subjects, you will find two ads on Page 4 of this issue. One is for the calendar and the other is for a webmaster. Once again we are looking for photographs for the calendar. If you have a photograph that you think meets the requirements please forward it to Peter. The second ad solicits individuals who would be interested in assisting Lex as Webmaster and eventually taking the job on themselves. Lex handles all of our Public Relations and Graphics needs and has been stretched extremely thin. If you have the appropriate skills, please contact Lex about this important position within CARM.

This is also the issue in which I will make my annual plea to the membership for articles. Layout articles particularly seem to be well received. I know most of you have been thinking of doing this but have just put it off, so today is the day to move this to the top of the must do list. Take some photos of your layout, write a brief article describing your modelling and send it off to me. Sharing members layouts is an important part of "The Canadian". The same applies to "how to" and construction articles. Send them in.

**THE EDITOR**

### PASSWORD CHANGE

**MEMBERS AREA: EFFECTIVE May 20th**

**USERNAME: grabiron**

**PASSWORD: railing**

### COVER PHOTOS

**Upper Photo: Ted Rafuse:** Jim Kemp's TH&B #1. The passenger car was built from a kit and described in a two part article in Canadian Railway Modeller in 2005.

**Lower Left Photo: Ted Rafuse:** The sun backlights CP #7401 and TH&B #56 as they idle on an industrial spur.

**Lower Right Photo: Ted Rafuse:** Modern power leads a train across the 3 foot long scratchbuilt deck girder bridge on Graham Reid's layout.

*Material for the Canadian should be sent to:*

**Editor: John Johnston, 41 Glenview Place, Hamilton, Ontario, L9C 6H9 or by e-mail at [editor@caorm.org](mailto:editor@caorm.org)**

# NATIONAL NEWS

The process of determining a governance model for CARM has taken a number of twists and turns. The majority of you expressed an interest in having a larger Board of Directors elected by the membership at large. However, the true test of such a model is having enough people step forward to run for the various offices. This did not occur. As a result, we again held a referendum of the membership in which we offered a number of separate choices. We have tabulated the 109 ballots which were cast and the results are:

**OPTION 1: No Change 15 VOTES**

**OPTION 2: Hold National Elections 30 VOTES**

**OPTION 3: Create an Advisory Board of Chapter Presidents 26 VOTES**

**OPTION 4: Appoint Provincial Directors 38 VOTES**

The first observation that can be made from this vote is that over 75% of you voted for an option that did not involve electing Directors. The second observation that I have drawn is that most of you have little interest in how the organization is run, other than it is run effectively. This is consistent with the message we have heard several times about keeping politics out of the hobby. On a positive note, I can tell you that two more individuals put their names forward as being willing to serve on a National Board. Based on the directions you have given to us, I have recommended the following structure to the National Board:

1. The following Officers of CARM be appointed by the membership at The Annual General Meeting for a two year term in each of the even years commencing in 2006. Position 1: Secretary/Treasurer, Position 2: Membership Director, Position 3: Public Relations Director, Position 4: Publications Director. The Public Relations Director will be responsible for all web related activities and the Publications Director will be responsible for the bi-monthly newsletter of the organization. These Officers will constitute the Executive Committee of CARM and may appoint such other individuals as necessary to work with them to carry out the day to day tasks of CARM.
2. The Executive will be responsible for appointing Provincial Directors for each of the following geographic areas: British Columbia, Alberta, Saskatchewan, Manitoba, Northern Ontario, Southwestern Ontario, Southeastern Ontario, Central Ontario, Quebec, Atlantic Canada, International.
3. Each Provincial Director will be responsible for assisting in the growth and development of CARM within their area of responsibility and in providing assistance to CARM members and CARM Chapters.
4. Each Provincial Director will be responsible for creating an Advisory Board in their area comprised of

the Presidents of the Chapters within their area and seeking input and advice from the Chapter Presidents on CARM activities.

5. The Executive will convene a minimum of two meetings per year of the full National Board with a goal of having four meetings per year. Meetings will be conducted by either conference call or Internet.

These advice of the membership as demonstrated by their votes and these recommendations put forward by the Chair were put before the National Board and approved at its meeting held on April 5, 2006 in Hamilton, Ontario. Over the next two months, members of the Executive will approach members to serve in the capacity of Provincial Directors.

I am certain that this outcome will not satisfy everyone, however, I recognized in starting this process to decentralize governance that satisfying everyone would be an impossible goal. The membership must also keep in mind that as a young organization, this is an evolving process. It would be my intention to revisit this issue in the summer of 2008 to determine if an electoral process had gained majority support. The Executive would also be open to individual geographic areas/Provinces establishing their own methods of electing/appointing Provincial Directors. Our best friend in this process will be an open mind.

## YEAR END STATEMENT 2005

### 2005 ACTUAL BALANCE SHEET

Revenue	Actual
Membership	\$19,757.69
Apparel	\$ 152.38
Calendar	\$ 942.00
Newsletter	\$ 2.75
Rebates	\$ 5,000.00
Bank Interest	\$ (5.67)
Convention	\$ 2,927.50
<b>Total Revenue</b>	<b>\$28,776.65</b>
Expenditures	Actual
Newsletter	\$ 6,728.61
Web/Pr	\$ 2,688.72
Membership	\$ 465.55
Gen Admin	\$ 532.88
Capital	\$ 856.49
Apparel	\$ 1,491.63
Chapters	\$ 4,809.56
Conventions	\$ 2,014.48
Calendars	\$ 1,491.63
Reserve	\$ 7,697.10
<b>Total Expenditures</b>	<b>\$28,776.65</b>

Respectfully submitted: Peter Moffett, Treasurer.

## Canadian Railway Craftsman Track Two—Web Site Contributor

by Dave Burroughs, CRC Manager

Track Two, as it is titled, Web Site Contributor, encourages you to submit your written work. This Track is an introduction to sharing your skills with all of the members of CARM through articles that may be published in The Canadian or on the CARM web site.

For some of you, the art of writing is a natural thing, for others, it may be more of a challenge. For those who do not feel comfortable writing an article, I encourage you to think of times when you are with your friends at the Model Railway Club, Chapter meet or at an operating or construction session with your friends. Think of things you have discussed. For example, you showed the guys at the club your new kit-bashed gas electric motor car, freight car or building. These “show and tell” sessions can be documented and submitted for publication. All you need to do is put your physical presentation into words and you are on your way to earning Track Two for your Canadian Railway Craftsman. Today’s word processor computer software makes writing easier by highlighting spelling mistakes and incorrect grammar.

You are required to submit material to the CARM web site based on modelling techniques and tips related to model railroading. A minimum of six articles for the web site is needed for credit in this category. Material may also be submitted for publication in The Canadian newsletter. Along with the application form, applicants should submit a copy of the articles that have been published on the web site or in The Canadian.

To be credited for this work, the material being claimed must be the work of the author. Web site articles must be all your own work and may contain text, photographs and diagrams. Published text, photographs and diagrams are eligible for this Track. There is a requirement for a minimum of 50% text for each article. Articles for the web site should be single spaced text and fill at least one screen. Published articles which are less than one screen can be combined to make up the total requirement of six screens. The minimum credit for partial screens is one quarter screen. Articles are required to be written in Microsoft Word, no special formatting is needed, and all pictures must be in jpeg format.

Over the last week or so, I have been watching the posts on the CARM Yahoo discussion group at <http://groups.yahoo.com/group/CAORM/> regarding how to make realistic roads for the model railroad. There have been some very good ideas put forth and I would suggest that you consider submitting an article to the webmaster for publication. Articles for the CARM web site can be sent via email to [webmaster@caorm.org](mailto:webmaster@caorm.org) or on a CD by regular mail to:

Webmaster, Canadian Association of Railway Modellers  
4296 Village Park Circle  
Beamsville, ON, L0R 1B8

Items for publication in The Canadian should be sent in the required format via email [editor@caorm.org](mailto:editor@caorm.org) or via regular mail to:

Editor, Canadian Association of Railway Modellers  
41 Glenview Place  
Hamilton, ON, L9C 6H9

In the next issue we will get everyone back in the comfort zone when we look at Track 3, Layout Builder.

Questions about the CRC program can be directed to [arc@caorm.org](mailto:arc@caorm.org) or by regular mail at:

CRC Manager, Canadian Association of Railway Modelers  
373 Cranbrook Drive  
Hamilton, ON, L9C 5W1

### 2007 CARM CALENDAR

#### THERE IS STILL TIME TO SUBMIT A PHOTO

Jpeg format 300 dpi or better

Or

Original colour photo or slide

**Include short description and name of the photographer**

**SEND MODEL OR PROTOTYPE SHOTS FOR CON-  
SIDERATION,**

Peter Moffett

Secretary, CARM

7 Jolie Court, St. Catharine's, ON  
L2M 6V5

**DEADLINE: JULY 31**

### VOLUNTEER REQUIRED FOR CARM WEBSITE

We are looking for a volunteer to assist with and then take over managing the Web Site. Several areas of the site will require weekly updates, other areas will be updated monthly and annually. The Web Site has already been designed and changes to the design are not required, only text and image edits will be needed.

You must have a working knowledge of Web editing and uploading to the server. Writing html is not required as the site is compatible with Adobe GoLive for editing. Software will be provided to the volunteer. An image editing program such as Adobe Photoshop is necessary to edit images.

The volunteer would start by assisting the current Webmaster who will guide him/her through the requirements and who would be available to advise the volunteer with any problems. This would be done with a view to assuming the role of Webmaster in the future.

If you have an interest, or require further information, please contact Lex Parker CRC MMR at [webmaster@caorm.org](mailto:webmaster@caorm.org)

# THE MODELLING OF GRAHAM REID

## Article and Photographs By Ted Rafuse

Graham Reid admits to living outside of Peterborough, Ontario, in the boondocks, but that has never limited his interest in watching trains and modelling trains. An indication perhaps that he is not a youngster is the knowledge that as a child he had a Lionel train set. Apparently it was the first such train in Peterborough and not only was Graham a proud operator, so too was his father. Commencing in 1947 the Lionel set enjoyed a topsy like existence that just continued to expand and expand. Regretfully now, all that Lionel equipment was sold and a move into HO took place.

Over the course of several decades about five various HO layouts were constructed all of which were never brought through to completion. As a retired cabinet maker perhaps he had to be secure in the knowledge that the bench work matched the life skill necessary to support the model railway. For whatever reason, Graham was never discouraged. Instead he discovered there was much personal enjoyment in scratch building structures, steam locomotives and rolling stock for his never completed layouts.

He is also somewhat of a photographer. He shoots primarily in slide format and has travelled the far and wide to photograph stations. His collection is a history that captured and now preserves many railway station that no longer exist. A testament to his interest was his scratch building a station following an Al Armitage article, "A Ticket to Tomahawk, Please," that appeared in the September 1961 issue of Model Railroader magazine. Graham recently built the station described in the article for one of the Christmas Contests that the Peterborough Model Railroaders stage for their own enjoyment. He has also scratch built a steam locomotive using an old Athearn chassis and completed with a modified Kemtron all-weather cab. The tender for locomotive 7931 was completely scratch built.

While his current layout is not completely finished, Graham nonetheless states happily that more has been completed at present than on any previous layout. The layout occupies three walls of a room such that one side is 13 feet, another 20 feet and a third 17 feet with a peninsula in one part of the room.



**Above: Tomahawk Station follows plans published in MR in the early 60's. It was built board by board and shingle by shingle. The roof is removable to view the interior.**

**Below: The climate allows for an outside saw at this logging mill and logs are piling up ready for cutting.**





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### BACK ISSUES AVAILABLE

Back issues of the Canadian are available and may be purchased for the cost of production and mailing. The costs are:

**Canadian Members:** 1 Issue—\$2.75 Each additional issue \$2.00

**U.S. Members:** 1 Issue—\$3.50 Each additional issue \$2.50

**European Members:** 1 issue—\$5.50 Each additional issue \$4.00

Costs are in Canadian dollars, the increased costs for U.S. and European members reflect the increased costs of mailing.

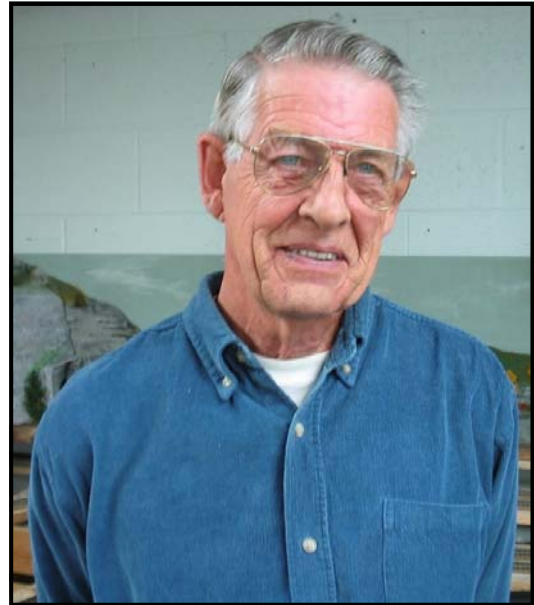
**TO ORDER BACK ISSUES CONTACT THE EDITOR**

In the remainder of the room are several work benches Graham uses for as his scratch building site.

Needless to say the bench work, basically of open L girder construction, is finely built. Scenery is partially complete as he works in a small section at a time. For the basic covering wire screen is used to form a basic landscape on which quick dry plaster is moulded. The basic white covering is then painted green and various Woodland Scenics materials used to complete the vegetation. Especially with the scenery, that activity is a family project with Graham's wife providing much of the direction and application.

The track plan is not convoluted and is basically a folded dog bone. Track is code 100 Peco with both Peco and Atlas turnouts. The turnouts are powered with either Tenshodo or Hammond & Morgan switch machines. The layout is electrically blocked using both rotary and double pole double throw centre off switches. Graham is quite happy to operate both Canadian National and Canadian Pacific diesel locomotives from manufacturers including Atlas and Bachmann Proto series.

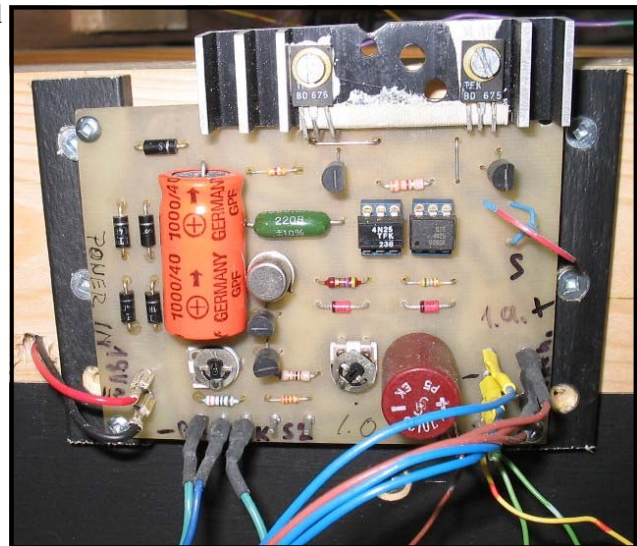
Scenery and scratch building sustain Graham's interest in model railroading. As far as operation is concerned his philosophy is: "I come down stairs and run trains."



**Above Right: Graham Reid**

**Left: Pictured is the transistor throttle that Graham builds. Four will ultimately provide the power for the layout. It consists of a 12 volt AC transformer which is converted to DC through a rectifier.**

**Below: CN #7931 is one of Graham's scratch built steam locomotives. He started with an Athearn chassis and built from there up save for the Kemtron all-weather cab. The entire tender was entirely scratch built.**



# MODIFYING “O” SCALE ROLLING STOCK TO TORONTO HAMILTON AND BUFFALO PROTOTYPES

Article and Photographs By Ted Rafuse-Models by Jim Kemp

Jim Kemp holds an ongoing admiration for the Toronto Hamilton and Buffalo Railway. As an O scale modeller for many years his modelling reflects his interest. A skilled and creative modeller, Jim however is not a slave to rivet counting. Rather he attempts to reproduce in miniature what appeals to him from the TH&B roster. As a guideline he uses purchased images or those which have appeared in various TH&B publications. At times his imagination leads him to create model rolling stock that the TH&B did not possess. As often as possible he fabricates what he needs from his bottomless parts box, painting and lettering his models as a finishing touch. One of Jim’s models, a snow plow, was featured in *The Canadian*, issue #7, Jan-Feb 2005. This article includes photographs of some of his model rolling stock with captions that briefly describe his modifications to the original plow model. Below through images and captions other rolling stock is described.



**Locomotive 4-4-0 #1:** The TH&B owned several 4-4-0 locomotives. Jim chose to model #1 and used a photograph as a guide to transform a Rivarossi locomotive into a TH&B model. Alterations included an extension to the boiler to resemble the original locomotive but the Rivarossi frame was not extended. The original gearbox and motor were removed and replaced with NWSL products. The headlight was scratch built and constant lighting added. The tender was not altered. New paint and lettering completed the model. The engine is part of a wooden passenger car train although it also occasionally works a small freight train. The wooden passenger car kit was described in a two part article in *Canadian Railway Modeller* in 2005.



## Steel Van #71

As the railways once did themselves, Jim created this car from a box car. An Intermountain steel box car provided the model base and his reference photo was located on the TH&B web site ([www.thbrailway.ca](http://www.thbrailway.ca)). The Intermountain model was selected as the side steel panels appeared to him to be the appropriate distance apart. The model was shortened by removing the side doors and associated panels. Special care was taken to ensure that one side of rivets was retained to appear on the rebuilt seam. Once the sides were rejoined, scratch built styrene ends, roof and cupola were added. End

railings came from modified Atlas caboose parts. The end ladders were scratch built. A lighting system similar to that in Van #69 was added to the model. Painting, lettering and window glazing completed the model.



**Wooden Van #69:** This metal van was purchased as junk at a flea market. First step in its alteration was the stripping of the original paint. The floor, sides and ends formed the base of the model. An overlay of scribed styrene was added to the ends and sides. An application of side styrene allowed Jim to model the entire side, including the wooden upper cupola extension, as one piece. With reference to a BRMNA publication photograph, appropriate window openings were cut in the side. Next the roof and cupola were scratch built from styrene. A smoke stack was also scratch built. In a similar manner styrene ends were fabricated and hand formed grab irons and end ladders added. Tomar marker lights were added. These are powered by a 1.5 volt battery inside the van. A slide switch under the van floor allows the marker lights to be turned on and off. Regular freight trucks were modified to resemble leaf springs found on most caboose trucks. Following a trip to the paint shop, acetate window glazing and dry transfer lettering were added.



**Hershey's Reefer #HREX 5021**

While rummaging through a box of old O scale kits at an eastern Ontario hobby store, Jim located a wooden reefer kit by Main Line. This kit may have been produced upwards of half a century ago. A few pieces of strip wood and cast parts were included, but most intriguing for him were the silk screen painted sides lettered for the Hershey Chocolate Company. As Jim had a Hershey Chocolate plant on his layout and the kit price was modest, the kit soon found its way to Jim's construction desk. Fortunately the kit instructions indicated the matching paint so that the unpainted portions of the wooden sides could be matched to the silk screened sides. With its printed billboard sides, the Hershey reefer represents an era from the first half of the twentieth century and fits in well with Jim's other rolling stock of that vintage.



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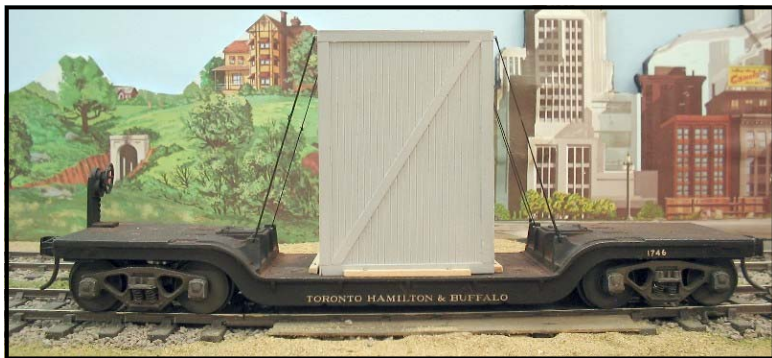


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**8 wheel depressed centre flat car #1746:** This Lionel depressed centre flat car was a junker Jim discovered at a flea market. Despite the damage, he liked the car because it was cast metal and the trucks were easy to remove making replacing the original wheel sets with scale wheel sets quite simple. The car was stripped of its original paint and repainted and relettered for the TH&B. The load was scratch built to represent a wooden crate containing an unknown piece of heavy equipment. Like its larger kin, this car also has no known TH&B credibility, except that it looks great and operates smoothly on Jim's layout.



**TH&B Reefer #5674:** No, the TH&B did not roster a wooden reefer but Jim thought the opportunity to build a wooden kit would be interesting and, when finished, this car would provide a reason to provide switching on his layout. The Juneco kit represents a CPR 45 foot express reefer. Jim largely followed the directions included with the kit adding only minor details. The paint shop provided the TH&B maroon colour to match his wooden 'varnish' of the same era. HO decals provided the railway name and car numbers. The kit comes without trucks and couplers; the former came from his bottomless parts box while the latter are KDs



**12 wheel depressed centre flat car #1825:** Jim had no references or illustrations when he began construction of a TH&B depressed centre flat car since the TH&B never had this piece of rolling stock. Undeterred Jim exercised his scratch building skills to construct a unique car for his model TH&B railway. Starting with .040 styrene Jim cut 4 over sized rectangular pieces, made a sandwich of the pieces, and glued the four corners together. He drew

the frame pattern on the top piece and then cut and sanded the sandwich ending up with four identical longitudinal frames, one for each side and the two inner pieces. Two end pieces were cut attached to an end. Spacers were cut to fit between the sides and the two internal members to ensure even spacing and create a stabilized rectangular frame. .020 styrene was used for the flooring. Cut to width as a single piece, this was contoured as one piece and glued to the frames in gradual increments. The glue was allowed to dry before attempting to conform the floor to the next section of the frame. Bolsters and coupler mounting pads were next scratch built from styrene sheet. Detail parts were added including brake wheels, hand made grab irons and tie down eyelets. The six wheel trucks came from Jim's amazing parts box – nothing is disposed of from previous projects! Paint and decal applications followed. The load was fabricated from sheet styrene and tied to the car with thread.

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# TORONTO HAMILTON & BUFFALO RAILWAY FREELANCED IN "S" SCALE BY RON SCOTT

ARTICLE AND PHOTOS BY TED RAFUSE

Ron Scott is reluctant to confess to the obvious, but he truly is a Toronto Hamilton & Buffalo Railway modeller. His Peterborough home layout features that railway's diesels, vans and some rolling stock, but he has yet to provide his layout with that company's distinctive name.

Modelling now in S.scale, Ron had several earlier layouts in other scales. As a child he was fortunate to own an AC Gilbert model train set. However even at a young age he moved beyond the traditional plywood layout and using code 148 track developed a scenicked, operating, AF layout of his own creation. Fortunately he retains some of this set on display in his current layout room. He occasionally adds rolling stock to this original set if his interest is peaked and if the price is right but he is not an AF collector.

Ron's entry into S-scale began with an eight foot switching section which allowed him to take this unit to train shows in the area. His current layout occupies an 11 by 18 foot area and is permanent although he is constructing a removable section which will allow him to participate as an S-scale display at local train shows.

The layout is constructed of 1 by 4 inch spruce braced boxes with risers supporting a base of styrofoam on top of which is Homasote, a sound deadening product. Ron enjoys hand laying rail, using HO code 100. For turnouts he primarily uses BK Enterprises switch kits, a few Shinohara turnouts and, for certain situations, builds his own turnouts as required. Electrically the layout follows a traditional block system. Ron employs a SoundTraxx System to which he has added on board speakers to three of his diesel units for added realism. This creates a marvellous sense of sound realism. All Ron's backgrounds have been painted by Peterborough friend Dave Mehew.

Products from S Helper Service, American Models, and scratch built units provide locomotive power on the layout. Many of the diesels have added directional lighting. To achieve this, Ron uses an LED coupled to a resistor. This simple method of illumination eliminates the more common multiple diodes and/or bridge rectifiers to light traditional bulbs. Rolling stock has come from S Helper Service, American Models, Pacific Rail and Ron has converted some American Flyer cars to conform to scale.



**Above:** An overall view of Ron's TH&B layout taken from the entrance to the room. The area under construction contrasts markedly with the extensive detailing which marks most of the layout. The skirt beneath the fascia ensures that attention is drawn to the surface of the layout as opposed to what might be below.

**Below:** A view down Barbara St. in Petticoat Jct. named after the popular 60's show captures the local TH&B switcher at work.



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As Ron modelled in HO earlier, he has used both modified and unmodified HO buildings. Fore ground HO models have had windows and doors altered to S-scale, while more distant unaltered HO buildings provide a forced perspective. Large HO structures are especially adaptable to altering. Other buildings are kit modified or scratch built as Ron enjoys the scratch building aspect of the hobby.

Switching is Ron's love in the hobby. The more, the better he states emphatically. His layout, due to room restrictions, was built for this purpose. Dave uses a card system to operate and he is not above making the switching a challenge for his fellow modellers when they are operating his layout.

Ron is a member of the Peterborough Model Railroaders group, as well as a subscriber to S Canada, TH&B Focus, CN Lines, CP Tracks, and by the time your read this, CARM. If you see Ron at a train show, he will be happy to explain why he believes S-scale is the best scale!



**Above: Ron is always upgrading his layout. This section will form a rebuilt Rockcroft Yard. Currently he is working on laying the track and turnouts by hand. Also in the photo are several pieces of rolling stock that Ron scratch built. Ron enjoys all aspects of the hands on approach to model railroading, one strong reason why he chose S scale.**



**Left: A telescopic lens bring the a portion of Wa-bash into close view. The Gulf Oil is a reference to denote the time frame of Ron's layout. A wayward GT van and several freight cars, including a TH&B box car are also visible.**

**Below Left: Ron built Rockcroft diesel #380 from an HO diesel chassis. All of the locomotive above the trucks and the frame attest to his skill and imagination in styrene fabrication. Rockcroft Shortline is a small railroad with a connection to the TH&B**

**Below Right: Ron Scott relaxing at home.**



## CHAPTER NEWS

**Editors Note:** If you have news about your Chapter, I need to receive it 30 days prior to the publication date of the Canadian. These dates for the entire year are: Feb 1, April 1, June 1, August 1, October 1, and December 1.

### ONTARIO CENTRAL CHAPTER:

On Saturday March 25 the Ontario Central Chapter held their 3rd meeting at Bill Osbornes home in Stouffville. There were 9 members in attendance. We discussed the success of the Copetown Show, our ongoing projects and the Montreal Convention. We also had a discussion about various modelling projects each member has been working on in the past couple of months. We then adjointed to Bill's basement where we observed his layout and we enjoyed refreshments as well as great fellowship and conversation about railroading. Our next chapter meeting will be on June 6th and it will be held at Bob Sparkes home in Orillia.



**ABOVE:** Members of the new Ontario Central Chapter display their Charter.



**ABOVE:** Part of Jim Osborne's layout visited during the Ontario Central Chapter meeting.

### ESSEX KENT CHAPTER:

The Essex Kent Chapter held a meeting February 12/06 at the Chatham "Superstore" meeting room on St. Clair St., in Chatham. There were 9 members and 3 guests were present. Outgoing President Gary Shurgold gave an clinic on "Tree Construction" from sedum. New officers of the Essex Kent Chapter for the coming two years are as follows: President - Don Eastman, Secretary Treasurer - Dave Ladore. Ideas were presented for future meetings and events.

**RIGHT:** Gary Shurgold gives a clinic on making trees from Sedum at the Essex Kent Chapter meeting.



Two new Chapters have been officially granted their Charters. Our congratulations to **THE LONDON AREA CHAPTER**, and **THE VANCOUVER ISLAND CHAPTER**.



CANADIAN RAILROAD HISTORICAL ASSOCIATION  
CANADIAN ASSOCIATION OF RAILWAY MODELLERS



**Montreal Convention 2006**

## SOME OF THE LAYOUTS OPEN FOR TOURING AT THE MONTREAL CONVENTION



Above: Canada Central Club HO & N scale layouts open Friday night during the meet and greet.

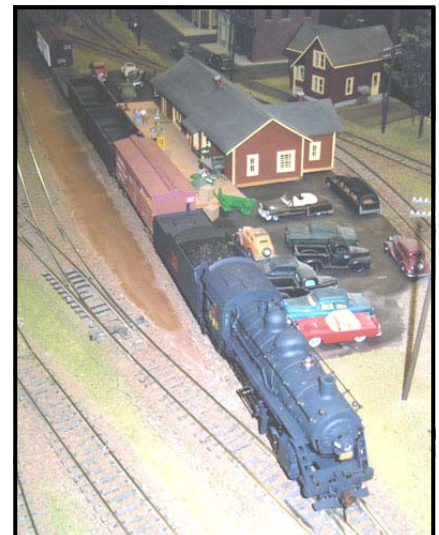


Above: British Model RR open for viewing on Sunday afternoon.

Below: Exporail HO layout open during Saturday afternoon



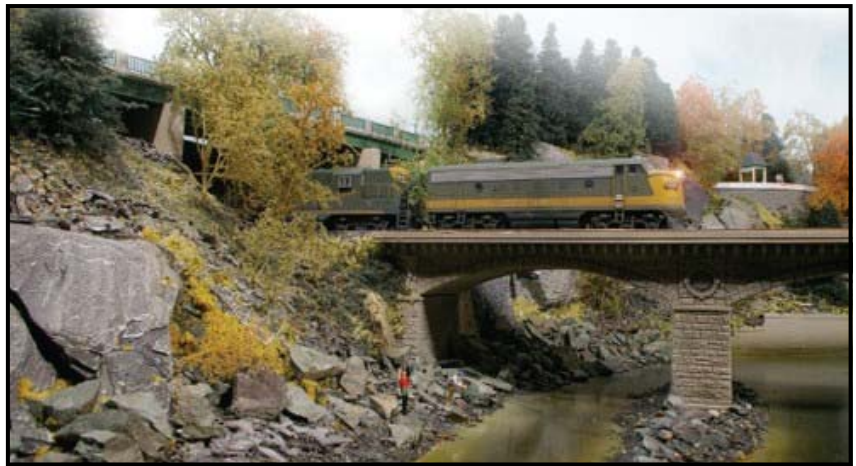
Below: Ken Bovingdon and Michel Fortin's HO layout open on Sunday afternoon





**Left: Pine Valley Club HO scale layout open Friday and Sunday afternoons.**

**Right: Vermont and Essex HO scale layout open Sunday afternoon**



**Left: West Island Modular RR Club HO scale layout will be open on Sunday afternoon**

# CRHA / CARM Montreal 2006 Convention Registration Form

(Please complete a separate registration form for each person.)

NAME  
ADDRESS  
CITY PROV. POSTAL CODE  
PHONE E-MAIL  
NAME FOR NAME TAG

You must be a member of one of the following groups or an immediate family member of one who is attending to participate or belong to one of the other groups involved with the convention, please specify. Check website for a complete listing.

(Check all that apply)

CRHA CARM BytownCN Sig CP Sig OVAR TRAQ  
Other:

Registration, prior to start of convention, (\$108.50 at the door): (a) \$ 98.50

Exporail Saturday Evening Banquet, \$48.50, (limited to 300): (b) \$  
(cocktails extra)

Total Due in Canadian Dollars: (a + b) = (c) \$  
(Only use this section if paying in US Dollars)

Add \$5.00 Canadian for new total (c + 5.00) = (d) \$

Multiply new total by 0.85 for USD amount (d) x .85 = (e) \$ US

CARM is handling the registration for this convention. Please make cheque payable to "Montreal 2006" and mail the cheque and this form(s) to:

Montreal 2006  
31-120 Quigley Road  
Hamilton, ON, L8K 6L4  
Canada

Registrations must be received at the Hamilton address no later than Friday May 12<sup>th</sup>, 2006 to be processed prior to the convention. Cancellations must be received at the Hamilton address by May 5<sup>th</sup>, 2006 for full refund, by May 17<sup>th</sup>, 2006 for full refund minus all food costs. No refunds after May 17<sup>th</sup>, 2006. Any refunds will be issued after the convention.

For up to date and more information check our website at [www.caorm.org](http://www.caorm.org) and follow the links to the convention site. For information or inquires about registration please e-mail David King at [membership@caorm.org](mailto:membership@caorm.org)

**Join us in Montreal on May 19, 20 and 21, 2006 to celebrate 170  
years of railroading in Canada!**

## COMING EVENTS

**May 5,6,7 and May 12,13,14: Medina, NY, Day out with Thomas:** Medina Railroad Museum 8 am to 6 pm.

**May 6/7: Lindsay, ON: 2006 32nd Annual Model Transportation Expo:** Sat 10 am to 5 pm, Sun. 10 am to 4:30 pm, Victoria Park Armouries, 210 Kent St W, for information contact Russ Moore at ldmrclub@hotmail.com

**May 6: Stayner, ON: 12th Annual Club Auction** sponsored by the Nottawasaga Model Railroad Club, St. Patricks Parish Hall, open for consignment items 8:30am, auction from noon to 5 pm. Admission \$3. For information contact Bill Payne, CARM #347-1 at 705-429-2762 or Dolf Roelofsen CARM #251-1 at 705-445-7317

**May 7: St. Thomas, ON, Railway Nostalgia Day** hosted by the Elgin County Railway Museum. This years event will be held at the new St. Thomas Elgin County Community Centre on Third Avenue just east of the Museum. 11 am till 4 pm

**Below: Ontario Northland F units rest in the sun outside the North Bay engine shops in the last week of May 2005. Photograph taken by John Johnston on the return trip from the CARM Convention in Winnipeg.**



**May 19, 20, 21: Montreal, Quebec, Celebrating 170 years of Railroading in Canada 1836 to 2006.** Sponsored by The Canadian Association of Railway Modellers and the Canadian Railway Historical Society. (see previous pages for registration form.

**May 27/28: Midland, ON: 18th Annual Model Railroad Show** sponsored by the Midland Model Railroad Club in the Midland Sports Complex, 472 King St. Sat. 10 am to 5pm, Sun. 10am to 4pm. 17 layouts, 60 vendor tables. Admission Family \$10 Adults \$5 Seniors \$4 Children \$2. For information contact Verne Jamieson 705-527-5307

**June 3: Stratford, ON: Stratford Railway Heritage Days** at the Kiwanis Centre on Lakeside Drive. 10am to 5 pm. Operating British and Canadian layouts. Historical artifacts from Stratford's railway past. Admission by donation. For information contact William Bickley at best@execulink.com

**July 21, 22, 23, 28, 29, 30: St. Thomas, ON: Day out with Thomas,** at the Elgin County Railway Museum. 8:00 am to 6:00 p.m.

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