

CARM Toronto Chapter - What are you working on?

April 4, 2020

Hello everyone:

I would like to say to all of you that the CARM Toronto chapter members are amazing! I have received many emails and photos of what everyone is working on and I knew it was time for another letter of *“What you are working on?”*. So, cheers to all of you! Go grab your favorite tea or coffee and enjoy reading about what some of our fellow chapter members have been doing over the last couple of months and what they are even doing right now as you read this!

When you are done reading, hopefully you too will be inspired to send your own story or pictures to encourage and help us all stay safe at home, isolated, and inspired to work on that project that (well quite frankly until now) *“you just did not have the time to work on it!”* Please, do send us something about *what you are working on?* I would like to get all of us on here at some time – that would be a great CARM Toronto Chapter feat!



And remember, this letter isn't about professional quality - if you want that there are plenty of magazines you can subscribe to; this letter is about all of us modelling with whatever we have available, with whatever resources we have, with whatever budget we have, with whatever time you can squeeze into this great pastime, and having fun doing so and staying in touch with each other as a membership. This letter is for us.

Got your tea or coffee ready? Excellent, turn the page and read on!

1. **Gerald Harper – New Portable Layout:** Gerald submitted this progress photo and writes:

The view is of a small portion of one module of a four module very light weight portable layout I am building to enable me to take it easily to shows. My previous portable layout - Anyox - had got too big and sophisticated so this new one - Par - St Blazey _ is coming along. It is four modules each 18 inches wide and 4 or 5 feet long constructed from aluminum angle extrusions and Styrofoam. No wood anywhere. It depicts a branchline that ran from a clay quarry to the coast to a small port in Cornwall. I have set it in 1929 and it combines O scale for the line to the port and On30 for the lines in the quarry. First display will be at the Narrow Gauge Show in Hamilton in April 2020 so I have a near term deadline for getting it finished.



Photo credit: Gerald Harper

2. **Hal Huhn – Continued Renovations For A New Third Level:** Hal has been renovating his Maerklin three tracks layout and sent us this new update on the work. When you get some time, re-read some of the previous letters to see where he started on this. I notice that Hal has a great new photo background already in place above the third level. Hal writes to us:

I am setting up my third level station. This requires some major renovation. It should be completed in 4 weeks.



Photo credits: Hal Huhn

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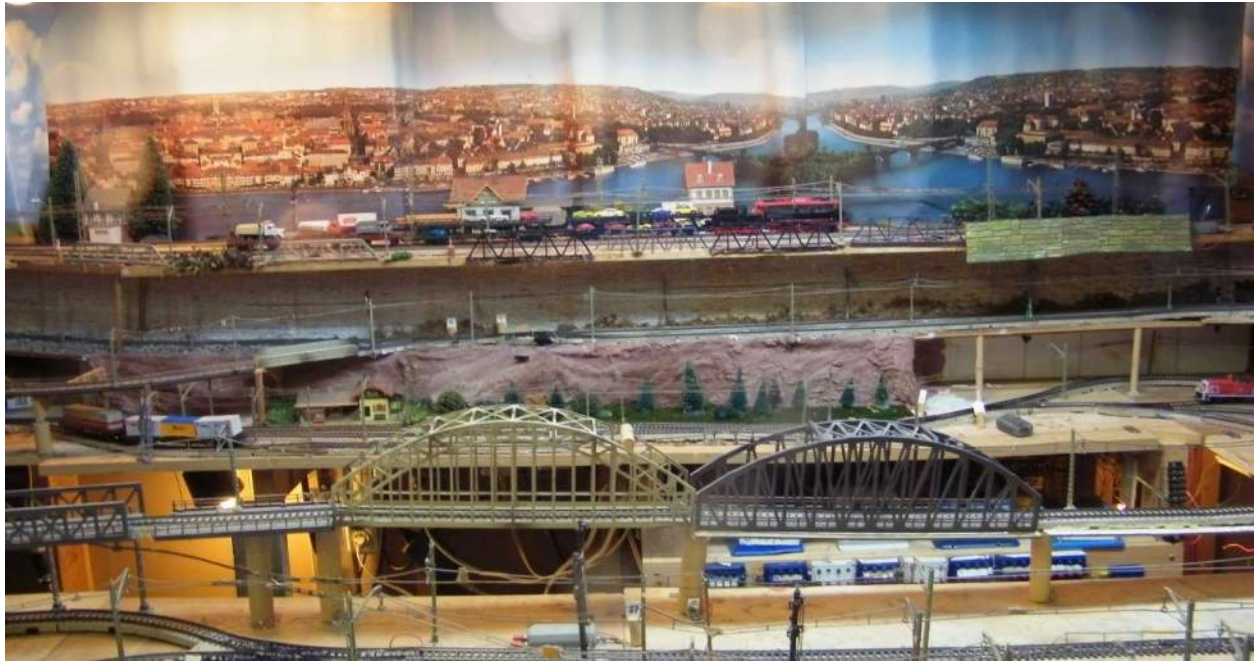


Photo credits: Hal Huhn

3. **Willie Waithe – Replacing Turnouts:** Operating sessions often bring some things to light that we did not previous note or that we would like to change. Here Willie writes to us that:

Two adjoining turnouts in the MacMillan yard were on an uneven surface because of ground sinking and it was embarrassing to see the locomotives shudder, bump and stall when attempting to traverse them. So, I dug up the whole thing (see attached image of the devastation), made a proper subroadbed support (1/8" fibreboard). I am now facing a one-hour trip to Credit Valley to get some code 55 #7 turnouts.



Photo credit: Willie Waithe

4. Mike Walton – Training Session: Both Willie Waithe and Mike Walton have been running some training session for members who have never actually had the opportunity (or very little) to participate in an actual operating model railroad session. That may seem odd to some of us, but I am indeed one of those people that, while I have been modelling and collecting my model trains for years and I am frequently found taking photos of maintenance of way trains by the side of the road or bridges, my life and choice of career moves have been such that I have only participated in two operating sessions (I don't count the ones where I run the MoW trains by myself on Willie's CN Weston layout on the weekends while I am working on buildings or landscapes – as fun as it is! 😊). There is nothing like the first time you get your own engine and your first throttle and you think "Now what?" I think running *training sessions* and *introduction operating sessions* are a great stress-free way to learn and include more of our membership in this great hobby and pass on the knowledge. I certainly have been grateful for Willie's mentoring in this hobby. Mentorship is for all ages and Mike writes to us about his training session as follows along with some great diagrams and charts:

This session was a special training session, our first one ever. While some started the day with trepidation, by the end of the day, much was learnt and a good time was had by all.

On order to focus on Network operation. Windermere was shut down to focus on network operations. In a sense we were going back to the days before the opening of Windermere (2005) and scheduled operation (2007).

The assignments were as shown below, the regular Network operators became trainers to the regular Windermere operators

		Network						Windermere										
		Lostock	Down line & Inner Loop	Outer Loop	Up line	Wigan / Horwich	Preston / Blackpool	Network Traffic Controller	Winderm. Traffic Controller	Station Master	Station Operator	MPM & Shed Operator	Goods Yard Operator					
214 Tning	AM	Gerry Taylor	Ken Sherwood	John Rowe	John Rowe	Alan Farmer	David Knight	Mike Walton	Network Operation Only									
		Paul Gauthier	Mark Earley	Richard Hall	Phil West	Keith Martel	David Youngs	None										
	PM	Gerry Taylor	Ken Sherwood	John Rowe	John Rowe	Alan Farmer	David Knight	Mike Walton						Trainers				
		Phil West	Paul Gauthier	Mark Earley	Richard Hall	David Youngs	Keith Martel	None										

Richard Hall joined the operators for the first time so his timing for joining the group was perfect.

Visiting trains.

Owner	Morning	Afternoon
Gerry Taylor	V1/3 2-6-2T with short parcels train	North British Class 16 diesel with short parcels train
Phil West	Lord Nelson Class (Sir Francis Drake) with 4 green coaches.	Lord Nelson Class (Sir Francis Drake) with 4 green coaches
Alan Farmer	BR 1F Round top tank with 2 push-pull carriages and Boston & Maine Doodlebug Trailer Coach	Class 31 with 2 push-pull carriages CP carriage

All visiting locos ran well but we had a number of problems from carriages with couplings with different heights. Gerry's Class 16 was an unusual locomotive and drew a lot of favourable attention.

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Lostock Locos

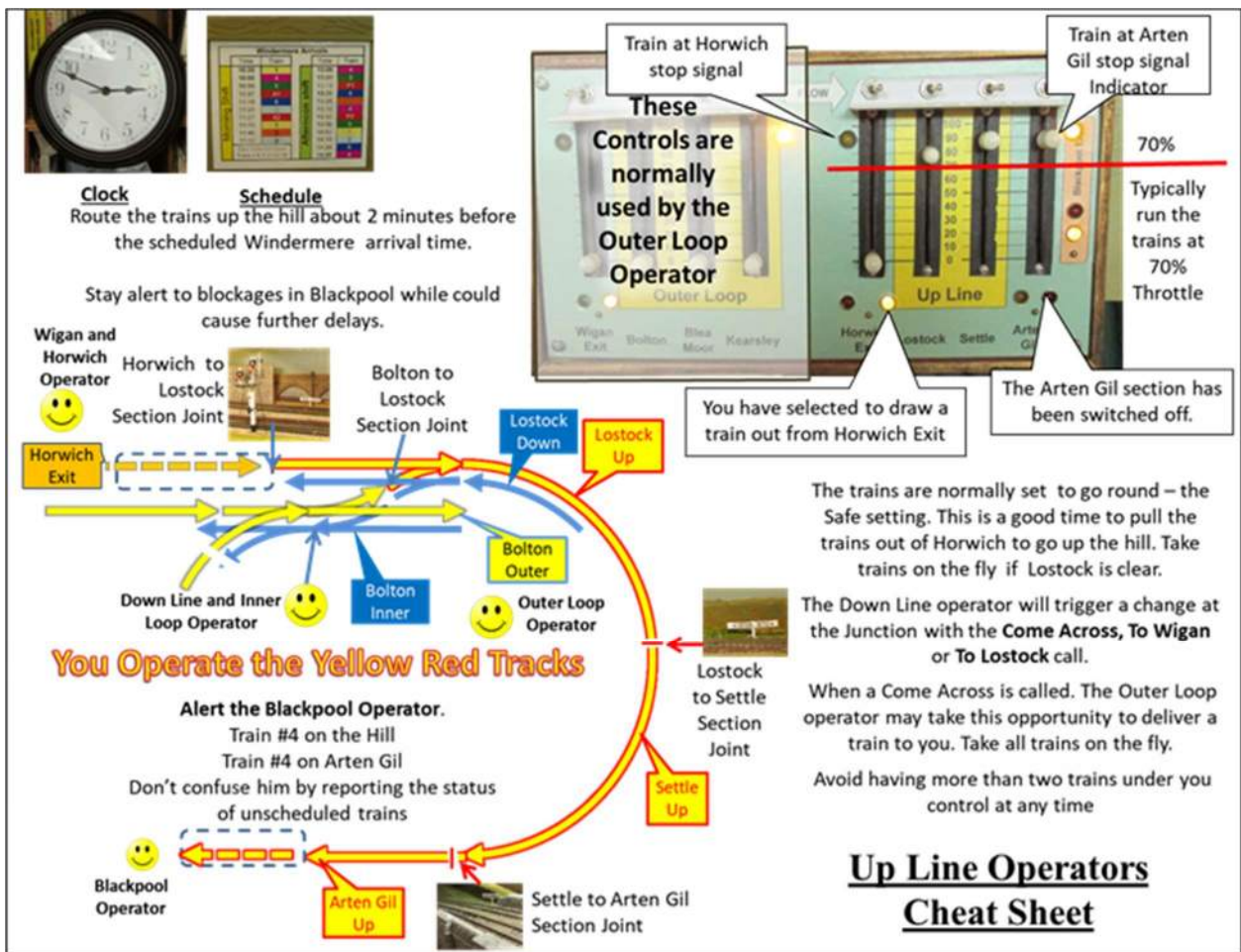
Lostock Roster for Session 214 Training Session

<p>XP1 4-6-2 6246</p>  <p>LMS City of Manchester</p> <p>Passenger Train #1</p>	<p>MT1 2-6-0 2715</p>  <p>LMS Horwich Crab</p> <p>Goods Train A1</p>	<p>HG1 2-6-6-2 4970</p>  <p>LMS Beyer Garratt - On Test</p> <p>Long Goods Loco #1</p>
<p>DMU</p>  <p>BR Railcar - 3 unit</p> <p>Passenger Train #3</p>	<p>MT2 2-6-0 2949</p>  <p>LMS Stanier Mogul</p> <p>Goods Train A2</p>	<p>HG3 0-8-0 9449</p>  <p>LNWR Class G2a</p> <p>Long Goods Loco #2</p>

We deliberately included the DMU to expose operators to the challenges imposed by "pusher" EMUs and DMUs, they found them ☺.

The only complaint, and it was a small one, was that there weren't any Jubilees.
The Training Procedure.

To carry some of this knowledge down to the new operators and as a future aid to returning operators, with help from the more experienced operators, I set up cheat sheets, one for each operator. They are all on a need to know basis, the example below is for Up Line operator.



As expected, some but by no means all of the information had been absorbed by the trainee operators. We didn't reach the level on competence to send the trains up the hill in the correct sequence but there were no more than normal collisions at Lostock. I was pleased to see that towards the end of the day the trainers were able to stand back and leave the operators to get on with it.

I think the biggest challenging the operators in the pit was the synchronisation of trains through the junction, I have taken the following words from the Cheat Sheets.

The Down Line operator normally triggers the change action with a Come Across, To Wigan or To Lostock call.

The Outer Loop and the Up Line Operators take advantage of this to Come Across, Exit Wigan or Exit Horwich.

If you imagine that all the trains coming down the hill from Windermere have no brakes and you must artfully dance around them, almost everything falls into place.

Rather than conduct a Windermere training session in the afternoon, as initially planned, we decided to continue with a second Network Training session after moving the students around to new assignments.

As one operator noted, a benefit of the Training sessions is that operators learn how their decisions impact all the other operations.

All photos, charts, and notes by Mike Walton

5. **Ian McIntosh – Rail Fanning:** I like to think Ian submitted this photo just for me because he knows I am nuts about anything to do with Maintenance of Way. He was traveling last fall and submitted this photo around February explaining in his email when and where he took the image:

In late September Joan and Ian McIntosh were in Port Hope and checked out the CN and CP viaducts, finding 8 men and 4 vehicles changing every tie on the westbound CP track, dozens of feet up.



Photo credit: Ian McIntosh

6. **James Rasor – When All Else Fails:** I work on the weekends as often as I can on Willie Waithe's new CN Weston Layout helping out here and there, learning new things, and building models as we need them. As an Architect, I love working on the buildings, and there are only a few buildings left from the old layout that need to be re-established on the new layout. Some of the old model buildings were against a wall on the old layout so they did not have a back wall to them. Now, on the new layout, their new location means they are visible all the way around the model wherever you look at it in the room so we need to add the missing wall(s), etc. That's easy for me, I review the original material and make a new façade for it, add new doors or windows, details, etc. and paint it match the original paint colours and voila! I've done this throughout the layout many times and with good success. I've also been painting my own models and doing watercolours for years. I always think I know my paint theories well and how to blend, match, mix, cool, warm, etc to get what I need. But this one stumped me. I just could not get the undertones to match the existing building. And I didn't want to repaint the entire building which was lovely. My solution?

I highly recommend matching the colour as close as you can, dry brushing a little onto the adjacent walls for gradation ... and add Ivy to distract the eye!



Photo credits: James Rasor

7. **Ian McIntosh – Convention (post poned):** Many members were working full speed on the Convention when it became necessary to postpone it with the current COVID-19 measures. I had a wonderful update for all of you on the Convention progress but we will now save that for when we announce its new date. In the meantime, Ian provided this update on the cancellation (for now!):

Once it was clear we needed to postpone the convention, the committee and other volunteers worked on three aspects:

- We informed our members and the public, including announcements sent with the paper and email Canadian, updates to the CARM web site, and elsewhere in model train event web sites.

- We cancelled the arrangements with Humber College, managing to get a full refund of our deposit and more importantly a full refund for all convention guests who had paid to stay in the college residence. We also sent refunds to everybody who had registered.

- We're also continuing most of the convention web site updates so it is almost set to go when the convention is re-announced.

Thank you, Ian, the committee, members, and all the volunteers who did all this work planning for the convention and then having to undo all the reservations and contracts. I look forward to registering when we can plan for a new date.

James Rasor

8. **Richard Morrison – Making Room:** We had a wonderful tour of Richard’s layout last year and he writes to us that we’ll be seeing some new work when we see his layout again. His layout seemed pretty complete during the tour and I wondered where he would find more space? Below is a picture I took of Richard at his layout during last March’s tour – keep an eye on the building above his hand for reference!



Photo credit: James Razor

Well, like Hal and others, he has found more space and he explains here how he found another 20 sq.ft of room:

After having run out of space for new structure kits I was accumulating, I looked around the layout room last fall and saw a corner occupied by a tool pegboard and rolling parts cabinet used for home maintenance. I made a low, rolling tool pegboard

and moved the tools there, then cut down the parts cabinet so both would roll under the layout, freeing up a 4' x 5' area. The expanded area features a mountain with a tunnel, a pond and a town site with room for many new factories and other structures. The layout will be shown during the CARM convention in Toronto in May. Here is a bunch of photos.



Photo credit: Richard Morrison



Photo credit: Richard Morrison

(On the left is the building that was above his hand in the first picture).

9. **David Hicks – City and Road Extensions:** David is renovating his layout to extend his city and the road using the method he explains below. I haven't seen his layout but I am sure glad he sent these pictures. I look forward to hopefully seeing it in the future! David writes:

The problem was a blank scenery divider with little space (4 inches) between it and the track. The solution was a cantilevered road extension over the track with low profile buildings & flats. With the exception of the Walthers apt, all buildings were kit bashed. The design preservation buildings were cut to 2 inches in depth. The backs were used elsewhere (another project). The retaining walls were made from moulds, the red brick is styrene sheets with Central Valley steel beams.



Photo credits: David Hicks

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10. **Mike Walton – Duck Weights:** Mike Walton, as some of you know by now via this letter, creates a fabulous write-up for each of his operating sessions. One of his readers had a question about one of his write ups and I was intrigued to. Question and answer below:

Question Mike What is that blue thing near the plaster?



Manufacture of Removable cover for Point motor at Preston Junction

1. A frame was fabricated to produce the desired contour
2. Burlap was hot glued to the frame around the edges
3. Packing tape was applied over the motor and micro switches to keep the burlap clear of the working parts.
4. Hydrocal was brushed onto the burlap and allowed to set.

Answer:

On Thu, 2 Apr 2020 at 20:02, Mike Walton wrote:

Hi Charles,
you shouldn't have asked that.

It is a duck weight from the Mould Loft at DeHavilland, see <https://en.wikipedia.org/wiki/Lofting>. They were declared surplus when they stopped using them following the transition to CAD and Catia.

I started my Engineering Apprenticeship in the Mould Loft at English Electric in Warton in 1959 so for me they have sentimental connections as well as being very useful at times.

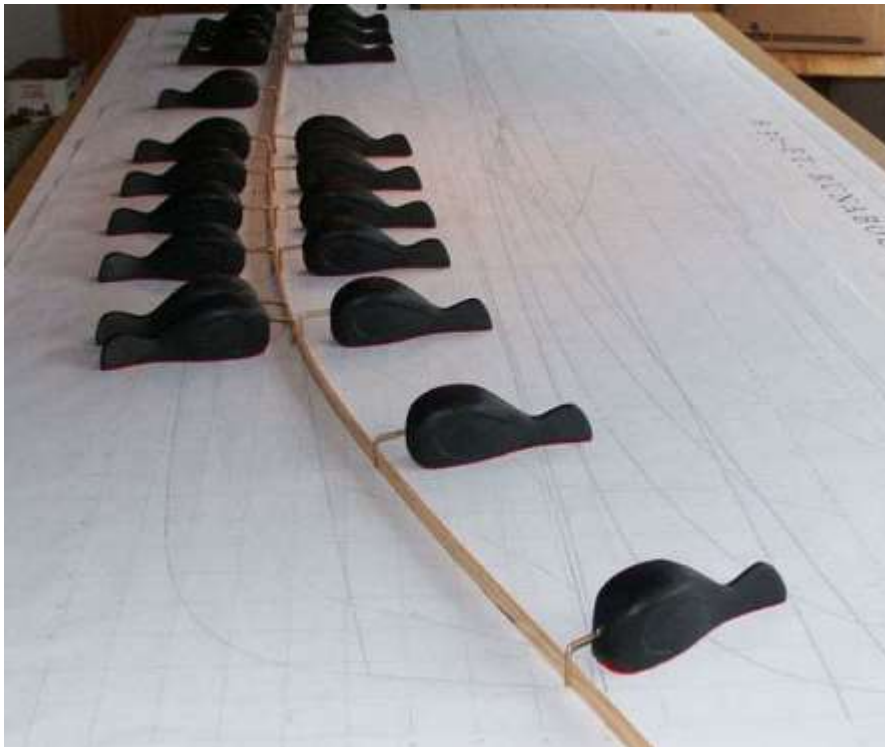


Photo credits: Mike Walton

11. **Willie Waithe – Escape Track:** Gerald, Bob, and Willie had an email series about what they were doing during the COVID-19 isolation and Willie wrote he was making an escape track - that got my attention! Willie writes:

I have also been "puttering around": My secondary booster gave out so I re-wired a section of the layout to by-pass it and, at the same time, changed the position of two PSX circuit breakers for better access (see photo).

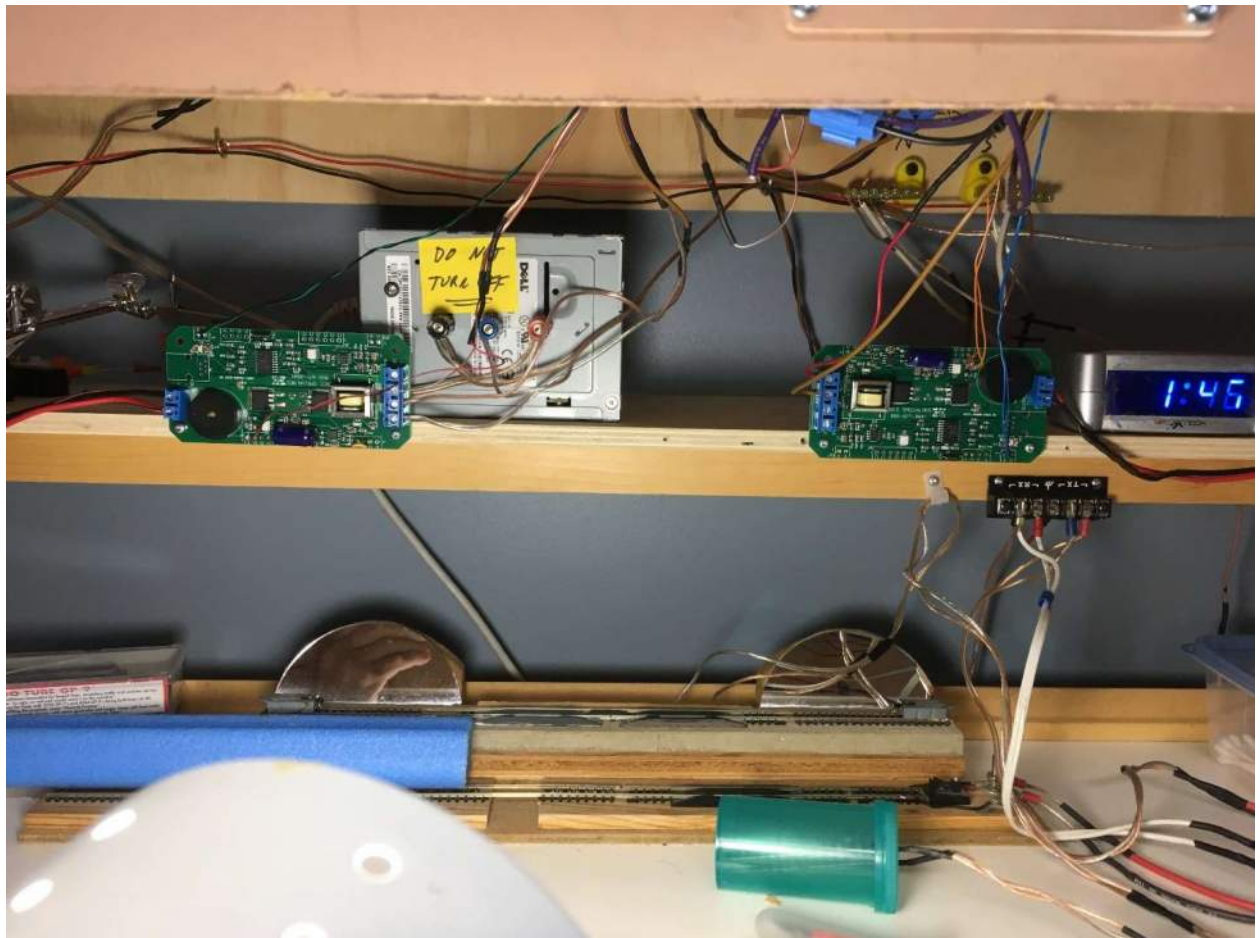


Photo credits: Willie Waithe

To prevent overheating of the main booster (it turns out that one may be sufficient after all), I increased the track voltage slightly and installed a cooling fan (photo).

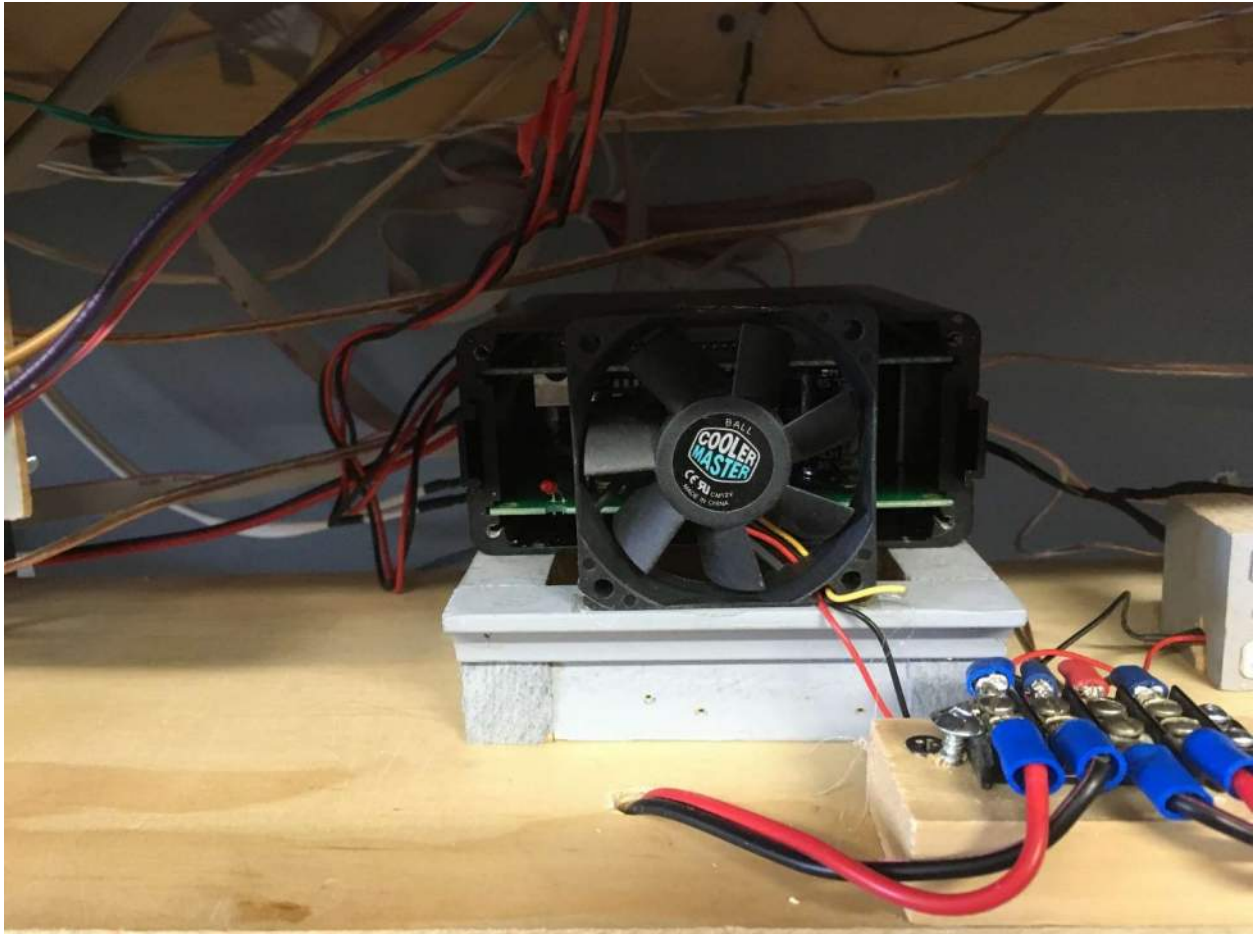


Photo credits: Willie Waithe

I am also installing a much-needed escape track for servicing the interchange yard to allow trains to clear the main track while switching the yard (photo).



Photo credits: Willie Waithe

I also repaired a "lumpy" set of turnouts in the MacMillan yard (photo)

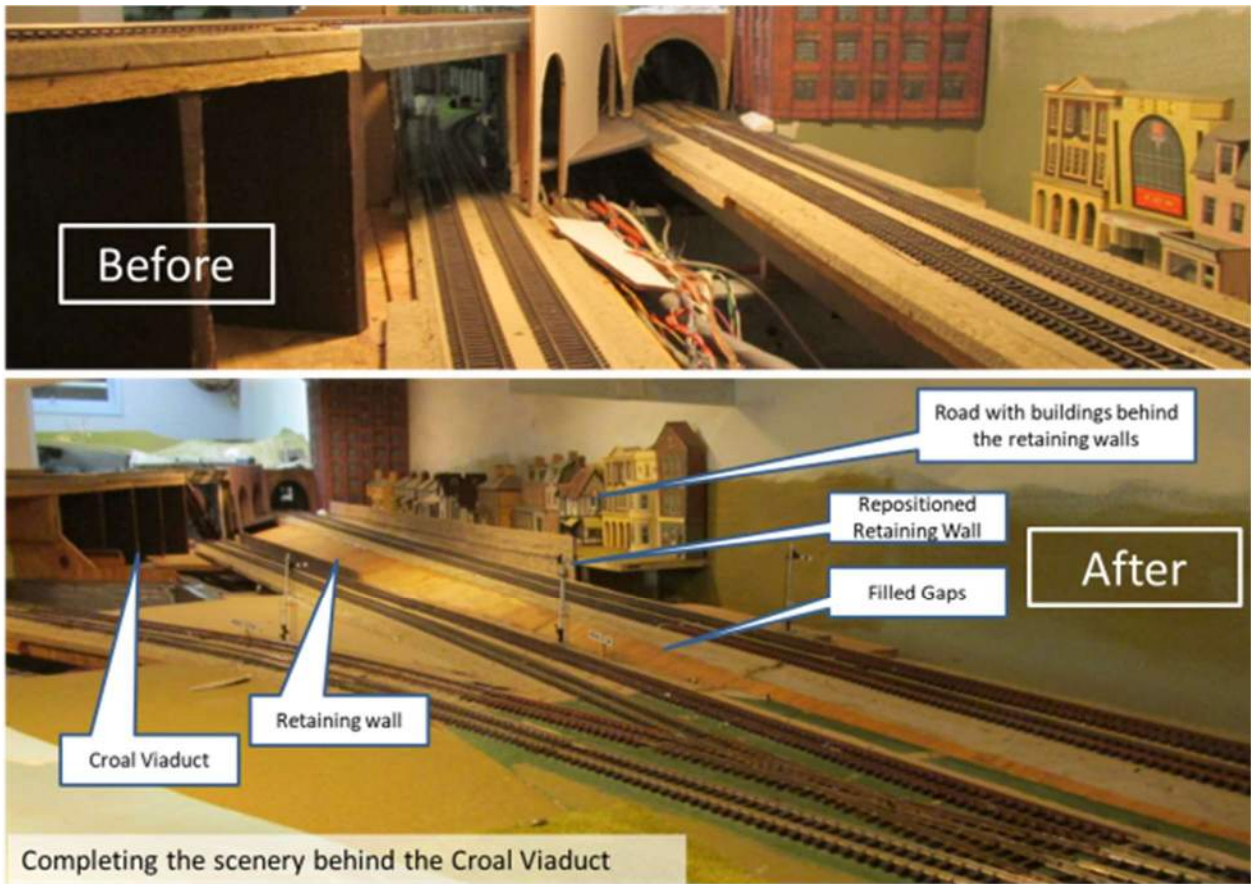


Photo credits: Willie Waithe

12. **Mike Walton – Croal Viaduct:** Mike is taking advantage of the downtime to do some changes too. Below are some excerpts from the beginning and end of his email on his new Croal Viaduct...

*Hi Folks
 I have taken advantage of the shutdown to conduct some major engineering works.*

Completing the scenery behind the Croal Viaduct.
To reduce the risk of damaging the viaduct at a later date I took some time out to do the heavy work on the scenery behind the viaduct.





You can never have too many clamps 😊



Text Session.

I'm not really in a rush to have the next session yet. It's been years since the scenery moved ahead so quickly,

I look forward to seeing more from Mike on this Croal Viaduct

13. **John Bigham – Highland Creek Sand Loader:** John sent a great photo of his latest project and I hope he keeps sending more photos on this progress. He writes:

James, here's a shot of my latest work-in-progress, an adaptation of the sand loader at Highland Creek Sand and Gravel from the '40s and '50s as the sole industry on a corner module, the 3rd of 6 finally to get scenicked. Over-the-tracks conveyor still to come has been inspired by the iconic Canada Crushed Stone conveyor over the CNR at Dundas, Ontario.



Photo credit: John Bigham

That's our April 2020 addition of what some of our CARM Toronto Chapter Members are currently working on.

Now, go look at your stash of projects, or dust off your layout, or grab that railroad book you have been meaning to read from cover to cover, or sort and label your railfanning work, or work on that railroad presentation, or install that first decoder, or write about the first train you ever saw or road on, or teach your family how to be operators or teach them how to solder or ballast a track, etc., etc., and have fun! It's one of the greatest pastimes that you can enjoy either alone, with friends, or with family ...and it keeps your mind active and healthy!

And.... don't forget, we would like to hear from every one of you about what our *CARM Toronto Chapter Members* have been working on so please do send me a photo and a quick 75 word maximum description (*or even just a description*) about what you are currently working on and we'll get it in the next **CARM Toronto Chapter - What are you working on?** chapter email letter.

How do you do it:

Step 1: Send me a single photo (*or even just a description*) of your:

- present model railroad project,
- or your most recent prized model railroad purchase,
- or your model railroad layout photo or layout drawing,
- or your most recent prized railfan photo,
- or your current model railroad scratch-built project,
- or your favourite model railroad rolling stock,
- or your latest model railroad operating session,
- or your research model railroad sketches,
- or your custom model railroad track build,
- or your first model railroad decoder install,
- or your latest model railroad weathering project,
- or the most recent model railroad disaster that happened to you,
- or your model railroad landscape project,
- or your model railroad lighting project,
- or the model railroad area you just pulled apart to start all over again!
- or any model railroad related item that you have been working on.

- It doesn't have to be perfect, professional, etc., etc., etc., just something you had fun with and want to share with us for this hobby we share together.

Step 2: Include a brief description, 75 words maximum; or less if you want, that's okay too!

Step 3: Don't have a camera? That's okay! Just send me a 75 word description of what you are working on. You do not always need a picture - the goal is to hear from all of you and we really do want to hear from you!

Step 4: Updates from you! Yes, we would love to have updates on items our fellow members have submitted previously for the letter – do let us know your progress since your last submission.

Step 5: Go ahead, submit something again! Did you submit last time? Wonderful! Please do so again and again and again! This is about us staying in touch and having fun together, even if it is just through this letter sometimes.

Step 6: Send it to me at: CARMtoronto.jamesrasor@gmail.com

I will then take the information, put it together with the other submissions and issue it in the next "***CARM Toronto Chapter - What are you working on?***" email letter along with the other members' news every few months.

Let us know what **you** are working on, have fun, and ***stay in touch!***

Yours truly,

James Rasor, Secretary

CARM Zone 2 - Toronto Chapter

For more information on the *Canadian Association of Railway Modellers* go to <http://www.caorm.org/>