



## CARM Toronto Chapter - What are you working on?

May 18, 2020

Hello everyone:

This truly has been an amazing couple of weeks. A few weeks ago, I had sent an email to all of you in the CARM Toronto Membership indicating that I would really like to hear from as many of you as I could in the next few weeks so that I could place your activities into this next letter that you are now about to read. Wow, you all really came through big time! I have received more submissions than I have ever had before and I greedily read through them all. I even had double submissions from some people! ... and then I ran out of weekend sorting and organizing them ...and then I needed more time to put all this together ...and I am not complaining, **not at all – you are terrific!**

And remember, this letter is about all of us modelling with whatever we have available, with whatever resources we have, with whatever budget we have, with whatever time we can squeeze into this great past-time, having fun while doing so, and staying in touch with each other as a membership. This letter is for all of us.

However, if all you have is a cup of tea ready for this report, then I am sorry to tell you this, but you are not ready. You are going to need an entire pot of tea with a tea pot warmer and the entire package of biscuits! This is big! This is fun! This is us! Enjoy reading and looking at some great CARM projects and initiatives by your fellow members!

Be well, be safe, enjoy the hobby, and *Happy Long Weekend* everyone!

1. **Ed Freeman – Modeling Rivers:** A few letters back Ed shared a bridge he was working on and I did not get it into the letter for which I sincerely apologize to Ed for missing; especially since I have always been thinking about it in the next letters and I was hoping to see an update from Ed on the progress. At the time, Ed wrote the following about modeling the river and bridge at the time:

*I have finally got around to modeling my river. It is a work in progress as more touch-up painting is required before the final coat of gel medium coating and ripples. Hopefully I will complete this mini-project before Christmas. Cheers*



Photo credit: Ed Freeman

*The bridge is not fully shown - it has a thro truss superstructure that I removed for ease of access while I work on the river bottom.*



Photo credit: Ed Freeman



2. **Richard McQuade – Cheese Car:** Richard submitted this modified cheese car to make it more prototypical and he writes the following about the process:



Photo credit: Richard McQuade

*I've been modifying a few cars to make them more prototypical. The cheese car is one example. It started life as a Roundhouse/MDC reefer. I shaved off the handrails and attached them as per the prototype, scratchbuilt the doors and vents in the corners, did full underbody detail and a few other things to it. I have a photo of the prototype car. Clare Gilbert designed the decals for Al Ferguson (based on the attached photo - the only one anybody has ever seen.) The car had a very lightly coloured body so my guess is yellow and not white. There's nobody around to tell me I'm wrong - a huge advantage to modelling 1914 when there wasn't colour photography and there's nobody left alive now who ever saw the prototype so no arguments there! I should have also mentioned that the couplers are by Sargent, the air hoses are Hi-Tech and the wheels are Reboxx semi-scale in the kit's trucks. The attached photo has been lightened a bit so that the trucks and underbody show a little better for the article.*

Richard also submitted the photo of the original car, which is shown on the next page.



Photo credit: Richard McQuade

3. **Hal Huhn – Station Blumenfeld On The New Third Level:** Hal has been renovating his Maerklin layout and sent us this new update on the new third level station. Hal writes to us:

*My third level Station Blumenfeld. To start this project I had to remove half of an inside wall. This caused dust, and a complete clean up of my whole layout. (My wife's suggestion of a plastic sheet came too late). The installation of the picture backdrop, and limited space, created a necessity of hugging the pillars.*



Photo credits: Hal Huhn



*The station consists of three tracks, each for two separate train operations, catenaries and third centre rails. There are two remote controlled signals. Both control the movement of trains, and catenaries and third centre rails. Some of the building had to be cut in halve, to accommodate the limited space.*



Photo credits: Hal Huhn

Hal ends his email by saying:

*My third level is almost finished. It will never be completely finished. I have attached two pictures and a short video. Since you are using e mail you may want to use the video.*

I've included Hal's video in the email for this letter for all of you to see.

4. **David Hicks – Part 1 City and Road Extension Continuation:** David sent this update to his last submission regarding the extension to his city and the road; here we see the completion of the new bridge on this side. David writes:

*Bridge finished except for weathering. I think I will call it the Covid Memorial*



Photo Credit: David Hicks



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*What are you working on?*



Photo Credit: David Hicks



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Photo Credit: David Hicks

5. **John Bigham – Highland Creek Sand Loader Update:** John sent a great photo of his latest project for the last CARM letter and here is an update on that building's progress. He writes:

*For the past month it's been mostly detail work on the sand pit loader module...and both the previous two as well: signage, fence lines, bumping posts, derails, semaphores, mounting servos, making the block occupancy detectors, power supplies, and programming to make them work. Plus the two conveyors to get the raw material up out of the pit and across the tracks, the scale house, a couple of retro-ed dump trucks, and a steam shovel down in the pit. Still have the Arduino sketch to finish, BODs to cut into the DCC drops, semaphores to set up, telegraph pole lines to install, and the lagoon to pour, so I need at least another month. As for the signals being on the wrong side of the track, there's a prototype for everything, in this case the northbound signal for the Burlington Beach swing bridge. Thank heaven for employee timetable special instructions.*



Photo credit: John Bigham

By the way, John, I hope I got your email address correct this time. Thank you for getting in touch with me and helping me to correct it.



6. **William Waithe – Escape Track:** Like many of you, Willie has been using the operation session downtime to make some adjustments to the CN Weston Subdivision layout. Here is one of the adjustments he made that he writes about:

*I hope you are all well. I am attaching a photograph of the completed "escape" track. It will serve as additional space for switching the Lambton yard and provide an escape for trains switching the yard as well as Lafarge cement and NorthWest freight transfer trains when the main track must be cleared for oncoming traffic. It's 110 cm. In N scale, one 50 foot boxcar or covered hopper (the longest on the layout) is 11 cm in length including coupler faces. Average car length of trains is about 9.5 cm., hence a capacity of 12 cars.*



Photo credit:  
William Waithe

7. **Ian McIntosh – Putting The Pieces Together:** Ian and Joan write that they have been working together to complete this kit:

*While staying isolated, Joan and Ian McIntosh assembled a kit for a logging train flat. It wasn't a shake-the-box kit, because it had 750 pieces and took two of us two days part time. The things that made it doable were that the pieces were all pre-cut and pre-coloured, and used tab-and-slot construction instead of glue.*



Photo credit: Ian McIntosh



8. **Richard Morrison – Building Factories:** Richard has just added a new extension to his layout. In the last letters I told everyone to keep an eye on a certain building as reference to see where he is adding to his layout – that building is in the far left of the photo below and it is also the feature picture for this month’s CARM May Calendar! Richard emails to me:

*James: Stuck at home for a month, I built three factories from Walthers plastic kits and a fourth from scratch parts. The Redpath Sugar plant is Walthers's Greatland Sugar, the Milwaukee Electric Tool Co. is Walthers's Milwaukee Beer & Ale, while Lakeside Shipping carries its original Walthers name. Wright Brothers Bicycles was from the scrap bin.*



Photo credits: Richard Morrison

9. **Ian Jameson – Freestyle Orley Station Layout:** Ian's layout is a freestyle layout with many themes and references on the layout via signage, building typology, landscaping, vehicles, etc to his consulting business in the food industry. He also has been working with lighting on the layout and just installed lights on his construction crane as well. Ian writes about his newest changes (and poltergeists?) in his email below:

*It's amazing that when you work on something on your train layout, things change on their own. Lights and controls that use to work suddenly stop working and then start working on their own. Anyway, just sounding off since it took WAY longer to finish what I wanted to do and send it to you.*

*Here is the update. I added a new development of four buildings near my Union Station train station on my HO scale freestyle Orley Station layout.*



Photo credits: Ian Jameson



*Two hotels, a police station and Broderick's bicycle store were added.*



Photo credits: Ian Jameson

*In addition, I have been concentrating on adding flashing lights and sound to the layout. I purchased some LED lights from Evans Designs in Colorado and added them to my fire response teams. The lights show up better in real time than in the photographs.*



Photo credits: Ian Jameson



*Roundhouse Design headquarters had some landscaping done and had a new access road built into the property.*



Photo credits: Ian Jameson

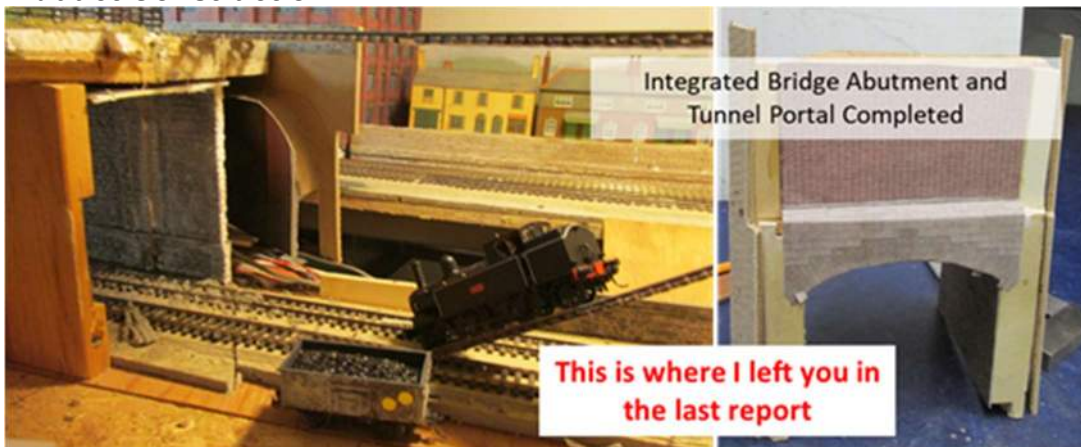
10. **Mike Walton – Croal Viaduct Continues:** Mike is taking advantage of the downtime to do some changes too. Below is an excerpt from his latest email on his new Croal Viaduct. It is too much fun to read, I could not cut it down shorter without doing injustice to it, so I have included all of the first part:

*Hi Folks*

*As under normal circumstances the operating session would have been held on April 21<sup>st</sup>, I thought it timely to put out a report on the work in progress.*

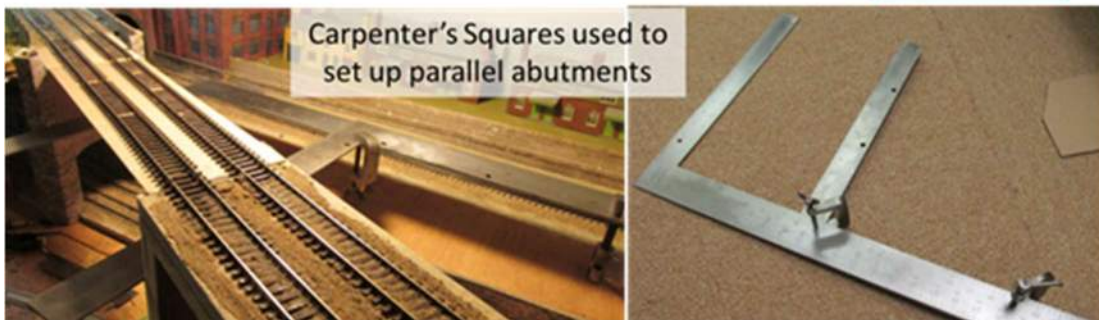
*Now is a good time to stay focused so I have changed my mind and decided to invest time developing additive manufacturing (3D printing) skills to recreate more accurately the cast iron bridge over the river Croal while continuing with the traditional methods elsewhere.*

**Viaduct Construction.**





Progress has been as follows

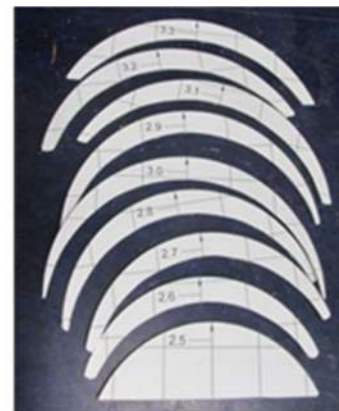


The Bridge will be preassembled so it's very important to set up the abutments accurately.



The arches were laid out in Draftsight to dimensions scaled from the photos

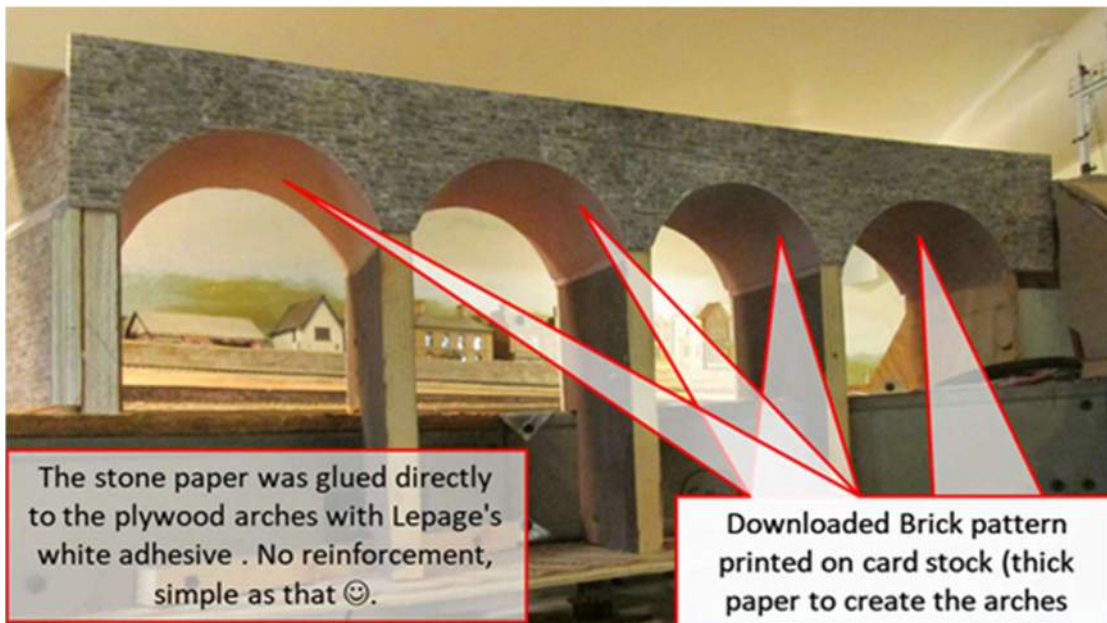
Suitable circle guides, 2.5" to 3.3" radius were made by gluing paper prints, produced in Draftsight to styrene sheet (similar to plasticard). These guides were used when marking out the arches on 1/8" plywood



*The arches were cut using a band saw and worked to the final contour with a belt sander.*

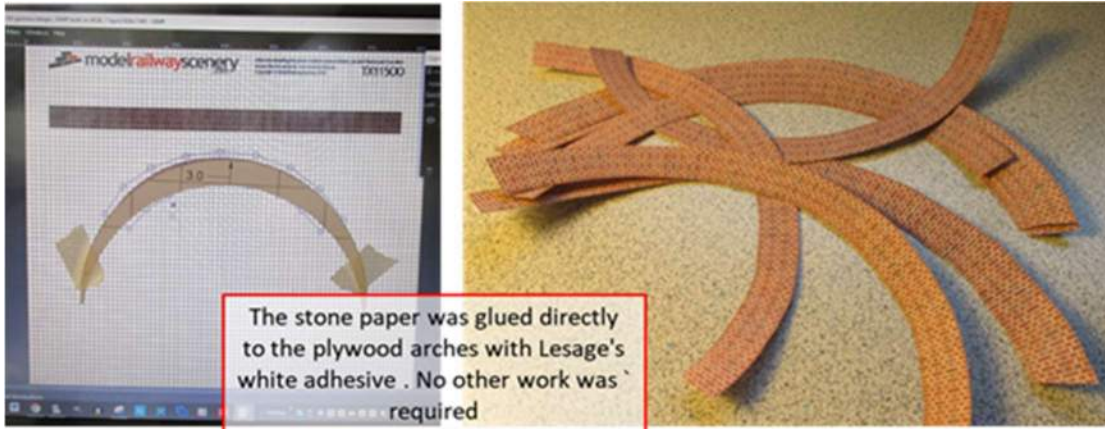


*After the glue set, the arch lining bricks were installed.*

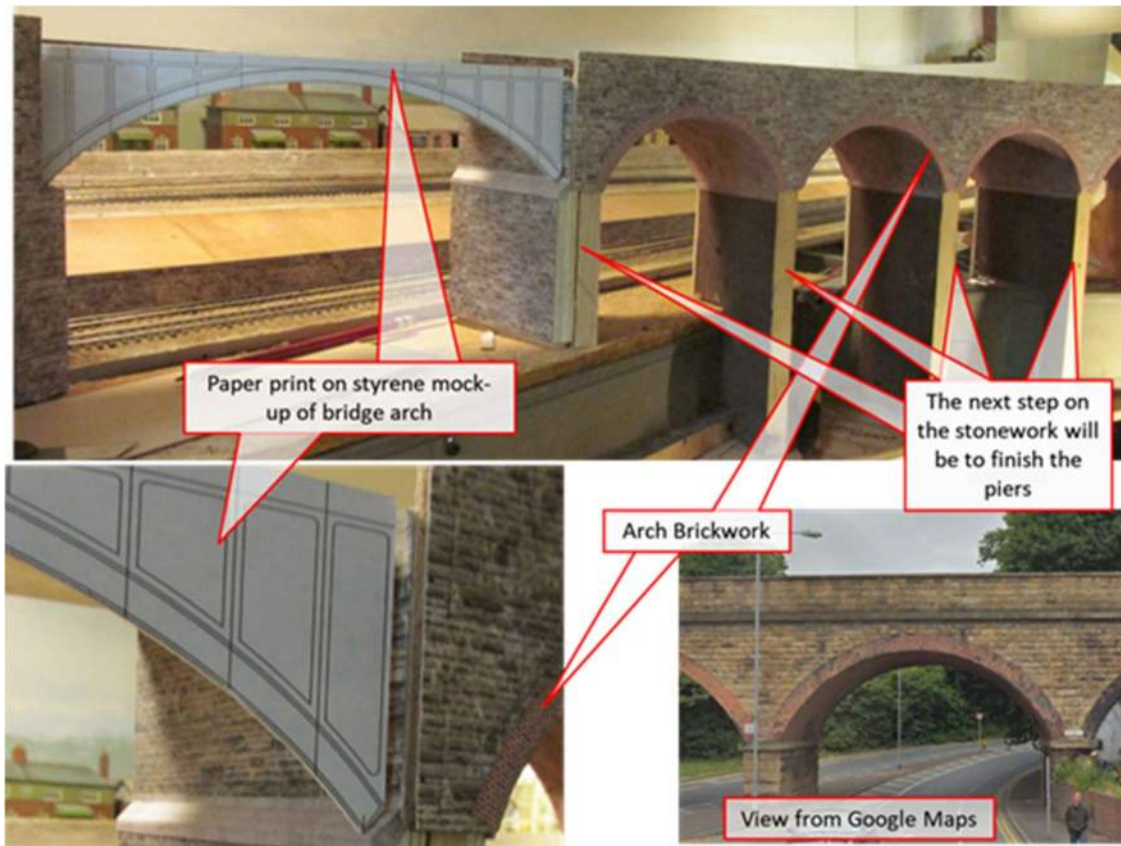




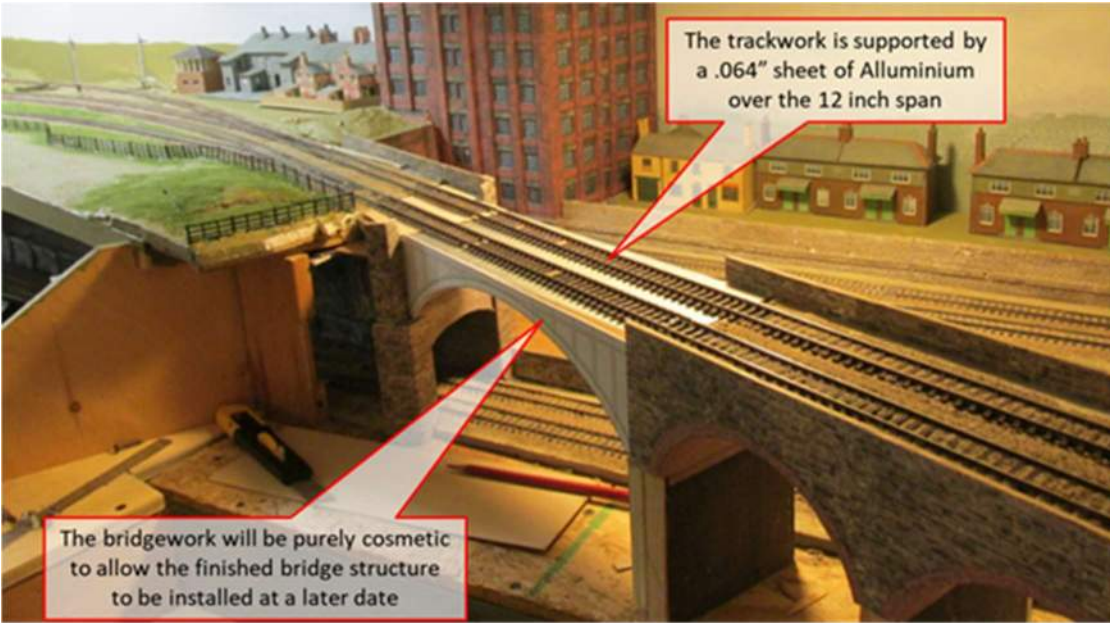
The Curved brick arches were made by manipulating the downloaded plain brick pattern. I used the Grid Transform function in the free GIMP (Graphic Image Manipulation Program) as reported previously.



The next step was to finish off the Arches, wallpapering skills applied J.



The next step was to reinstall the track.







*After use I clean out the syringe thoroughly and apply a touch of Vaseline to the plunger seal.*

All photos, diagrams, report, by Mike Walton.

11. **Andrew Malette – Trackwork On The Lower Level:** I was very glad to see Andrew give this us update on his layout. We showed a detail photo of his work last February in our CARM letter where he had finished the hand laid track work and cut the rail for the lift out that I personally admired very much. Andrew give us this update:

*I have started trackwork on the lower level of the layout. These pics show the Owen Sound Mainline, passing siding and storage/Supertest tracks just north of Owen Sound terminal. The boarding tracks for the grain boxcars will be going in on the right.*



Photo credit: Andrew Malette



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*What are you working on?*



Photo credit: Andrew Malette



Photo credit: Andrew Malette



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Photo credit: Andrew Malette

12. **Gerald Harper – Two Brass Projects:** Gerald has been working on several projects and this is the first of two projects he has sent for us! Reading about his construction of the second ore car below has made me promise to never complain to myself about soldering two simple power leads to tracks ever again. I look forward to more posts from Gerald on finishing these two pieces. Gerald writes:

*Here are my two brass projects. Both are On3 for my Anyox layout. The loco is a second 0-6-0 ST which will sit on a Bachmann drive widened to appropriate gauge. It was held up for a while by waiting for PSC castings but back in construction now.*

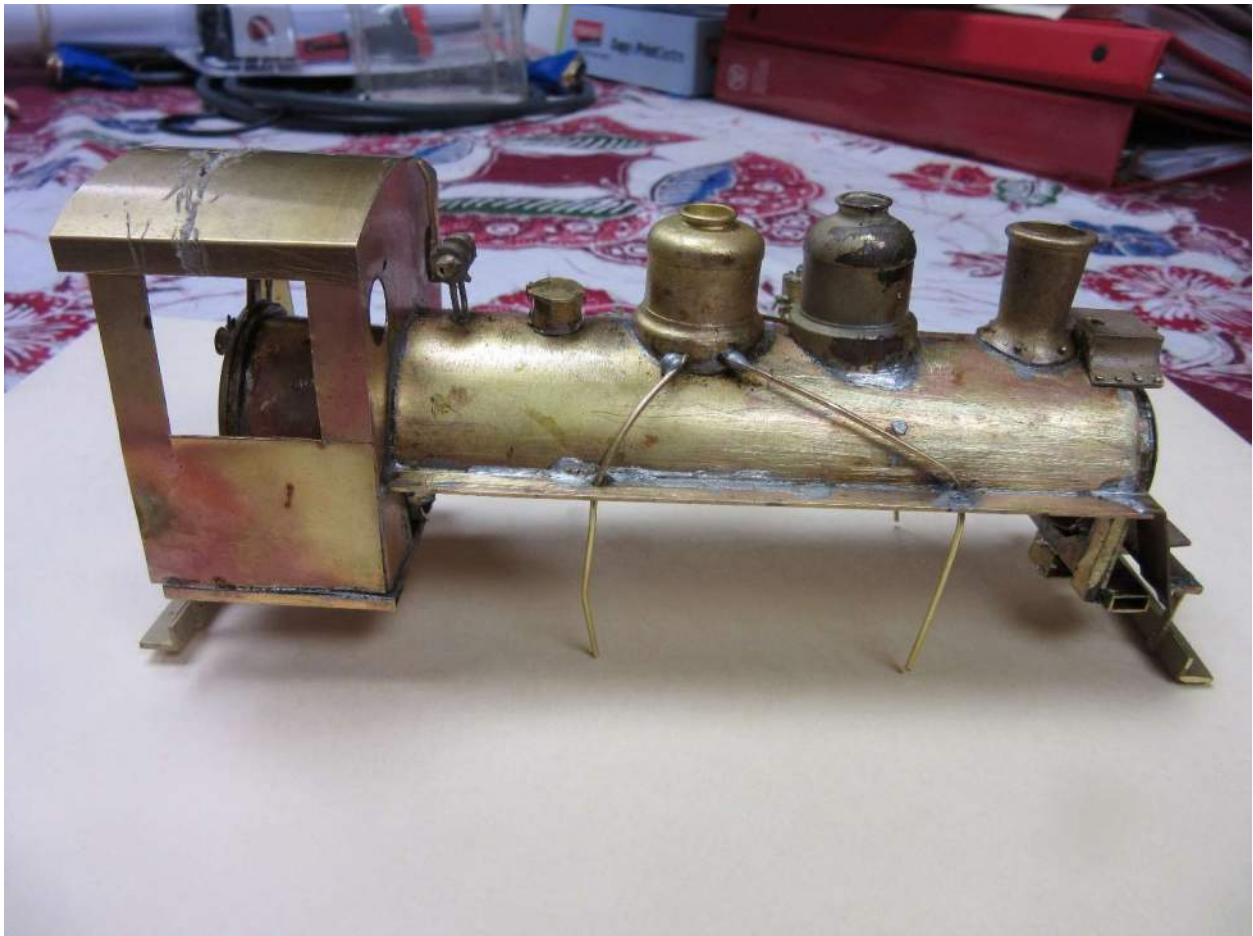


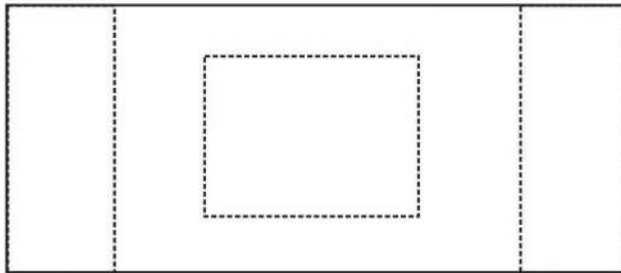
Photo credit: Gerald Harper

*The other is one of the Anyox ore cars built by Pacific Car and Foundry in 1914 and for which plans are in the appendix at the back of Muralts book on the Dolly Varden RR.*

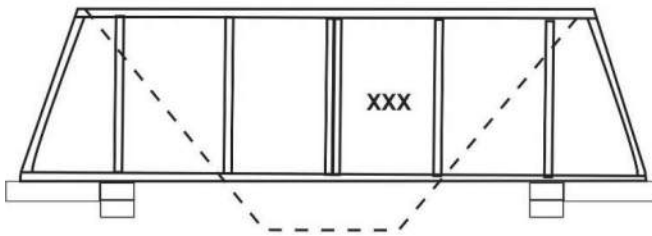


I have drawn up a detailed plan with lots of notes.

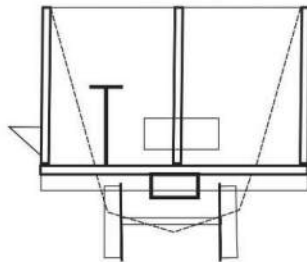
GRANBY CONSOLIDATED MINING, SMELTING AND POWER COMPANY  
Anyox Mine 200 series steel ore car, the "COVID" Car.  
Built by Pacific Car and Foundry Co, Renton, WA., 1915.



Base plate, brass sheet, (e.g. K&S #8249, 0.064 x 2" / 1.63 x 50.8mm).  
cut hole for centre hopper  
recessing, dashed lines  
are centre lines for bolsters



Side panels x 2,  
brass sheet (e.g. K&S #250, 0.005",  
with angle strip (e.g. K&S #9880 1/8")  
ribs. Dashed line represent  
position of inside ore box sheet  
needed for rivet embossing. Bolster  
bars are 2 x 1/4" x 1/8" rectangular  
brass tube such as K&S #8264. Height  
should be checked against Kadee Coupler  
Height Gauge. Number is 200 - 240 in white,  
Aerial Bold font.



B end view with brake wheel,  
air reservoir  
and walkway brackets for timber walkway.  
Brass box section soldered under base plate  
to take Kadee #803 couplers is 0.35 x 0.15"  
e.g. K&S #8266. Trucks should be arch  
bar, 3/7" wheel base with 26" diam.  
wheels. Cut a styrene shim to fit inside  
coupler box out of 0.065" sheet so that a  
Kadee #803 coupler will then fit snugly and  
not flop downwards. Shim can be above  
or below couplers depending on height  
gauge needs.

Cutting outline for sloping sides of ore bin - cut 2 of each from 0.005" brass sheet such as K&S #250. Builder may cut these oversize and create tabs to fold over and assist in strengthening soldered "funnel" or use 1/8" brass angle.

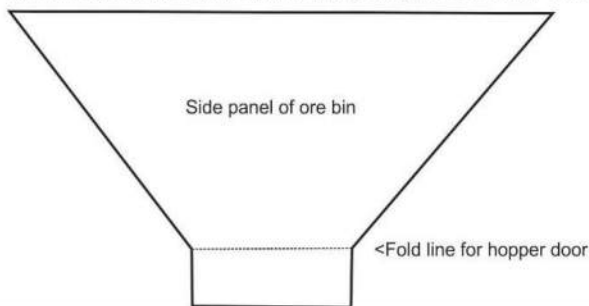


Photo credit: Gerald Harper

*Right now the car is halted waiting for a supply of brass sheet and angle brass. I tried making angle brass with a bending brake which worked fine for the vertical ribs but cannot make the long horizontal ones. Then I ran out of brass sheet as well. The detailed plan is to actual scale so I can use it as a cutting template for the conical rock box. Took a lot of trial and error to get those sizes correct. Still one more to do. Anyox ran 8 car ore trains so I need more than one car and if this first one works out I have a contact who I am going to ask to laser cut me the parts for several more cars for an ultimate fleet of 4+.*



Photo credit: Gerald Harper

*Soldering the ore car sides is a pain because every time you reheat to add an extra rib you risk loosening the surrounding ones so you need multiple holders to keep them all in place while it cools back down again.*



**13. William Waithe – MacMillan Yard Repairs:** In our last letter, Willie told us about some changes he wanted to do for some turnouts in MacMillan Yard after observing the locomotives passing over them in the last operating sessions. Willie give us an update on the progress here and writes:

*Since the last report of 4 April, I have completed the repair in the MacMillan yard: The two turnouts have replaced on a solid base of 2" styrofoam, overlaid with 1/8" hardboard. With everything sealed, the area has been ballasted and landscaped. The next step: I now notice that there are vehicles in the yard but there is no through road for them to get there!*

1. *The prepared surface, leveled and re-enforced. Note the servo assemblies attached to the two #7 Atlas code 55 turnouts.*



Photo Credit: William Waithe

2. The turnout assembly in place. The servos have been connected to their controllers and the frogs have been connected to their respective frog juicers.



*Photo Credit: William Waithe*



3. After sealing openings, ballasting and landscaping.



*Photo Credit: William Waithe*

14. **Sim Brigden – Zoom Meetings:** Sim asks if CARM would consider doing an online Zoom meeting, writing to us:

*Though some of us may be technologically challenged, may I suggest that CARM sets up a Zoom Meeting and sends out invitations to all members. I was recently in another Zoom Meeting on a completely different topic and it was most refreshing to see what people were up to. There were some 25+ of us and it was a lot of fun.*

Yes, Sim, we are looking into the logistics of hosting an online meeting for CARM members.

15. **David Dunning – Armchair Railroading:** David is doing what many of us are doing and writes:

*I like your idea and I have been doing armchair railroading do to lack of space.*

16. **James Razor – Toronto Themed Armchair Railroading:** If you are looking for some Toronto themed railway information for you or your family, I would like to suggest that you also look at the Toronto Railway Museum website (TRM). Their museum manager, staff, and volunteers have been working really hard to bring their rolling stock collection, collective works, and their new commissioned works to the public via the internet during these isolated times. Check out these links:

- TRM's **virtual model train show:**
  - [https://www.facebook.com/TORailwayMuseum/posts/2795252787211008?\\_tn=-R&mc\\_cid=36d6f369d4&mc\\_eid=954a563dcf](https://www.facebook.com/TORailwayMuseum/posts/2795252787211008?_tn=-R&mc_cid=36d6f369d4&mc_eid=954a563dcf)
- TRM's new **exhibit on Union Station:**
  - <https://www.uncoveringuniontrm.com/>
- TRM's series of **jigsaw puzzles** based on their collection:
  - <https://www.jigsawplanet.com/TORailwayMuseum>
- TRM's May 24 **scavenger hunt:**
  - <https://torontorailwaymuseum.com/trm2018/railway-museum-online-savenger-hunt/>
- TRM's **Audio Guide** of the museum's collection:
  - <https://torontorailwaymuseum.com/trm2018/audio-guide/>
- The Toronto Railway Museum also produces a **very informative monthly newsletter** which is targeted at their visiting public. If you wish to get these monthly, just sign up at:
  - <https://torontorailwaymuseum.com/trm2018/newsletter/>



17. **David Hicks – Part 2 City and Road Extension Continuation:** David sent this second update to his last submission regarding the extension to his city and the road; here we see the completion of the other side of the City. David writes:

*Finally finished the city side of the peninsula. was inspired by the Dunedin N.Z. Station. Going to run some trains for a while before I start the next project.  
Dave*



Photo credit: David Hicks



Photo credit: David Hicks





Photo credit: David Hicks

18. **Malcolm Back – On30 Backwoods Switching Layout:** Malcolm sent us this new update on the layout and benchwork that he has been working on. We gives us a good description of the work we see in the photos as follows:

*I have been busy building the bench-work and laying the track for my On30 backwoods switching layout. The scenic part will be 12' x 2' with a 5' x 2'6" L. There is also a 5' x 1' hidden staging yard. The L will have a sawmill, log pond and dump, and an ore transfer dump from a mining branch. The control system is Digitrax DCC. If you look at the 2018 On30 Annual, San Juan Junction Railway pp 36-41, this is the type of Railway I envision.*

*Engine terminal and 3 track staging yard in the background*



Photo credit: Malcolm Back



*Future Sawmill, Log pond and dump*

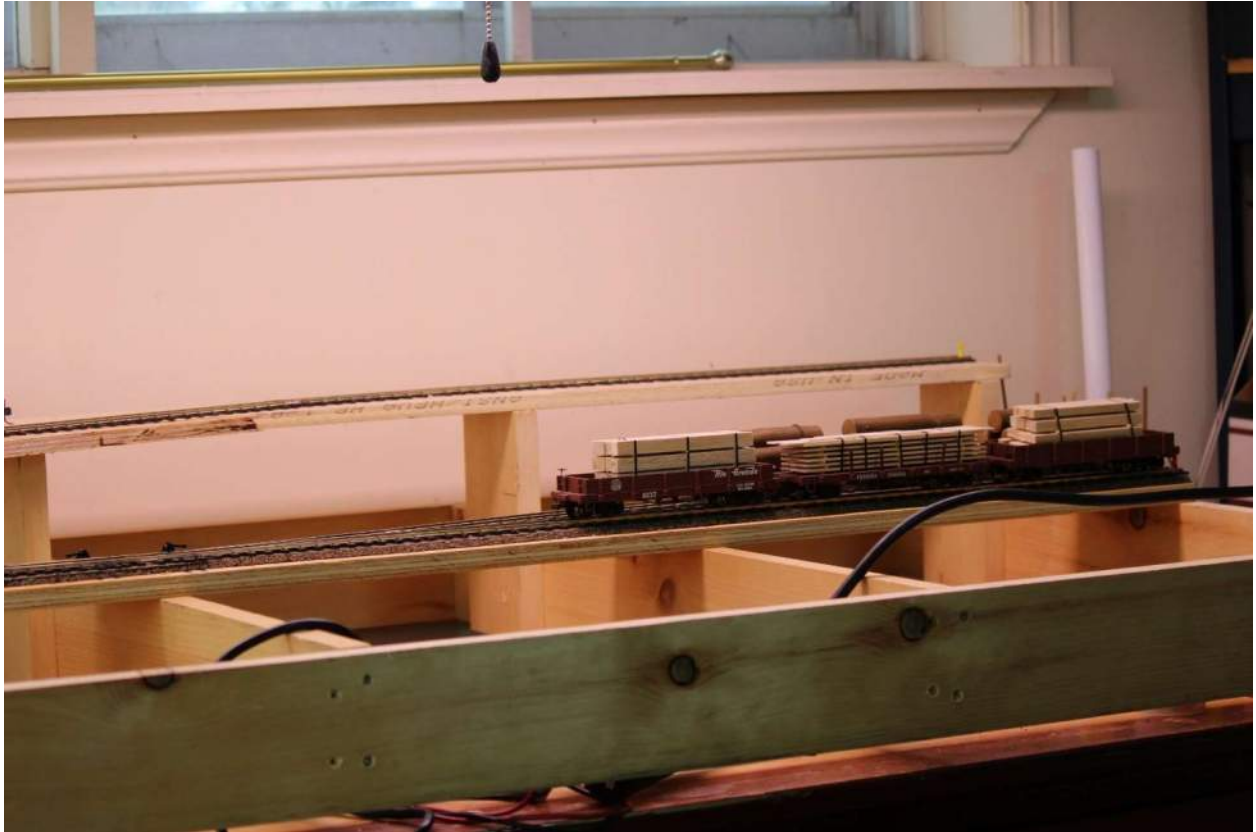


Photo credit: Malcolm Back

*Mine dump and industrial area and Yard*



Photo credit: Malcolm Back



*Engine terminal*



Photo credit: Malcolm Back

*3 industrial tracks and 2 yard tracks*



Photo credit: Malcolm Back



*Over-all shot of most of scenic area, lumber mill and log dump off to the right in distance*



*Photo credit: Malcolm Back*

*Yard area and Industrial switching area*



*Photo credit: Malcolm Back*



*3 track hidden staging*



*Photo credit: Malcolm Back*

*On3 coach converted to On30*



*Photo credit: Malcolm Back*

*Shay locomotive on turntable*



*Photo credit: Malcolm Back*



*Water tower*



*Photo credit: Malcolm Back*

19. **Harold Kemp – Atlas Lumber Yard Kit:** Harold sent this great posting on the resurrection of an Atlas Lumber Yard kit he has. First, he writes:

*Hi guys. You may remember I posted a wee project about resurrecting an old lumber rack kit. I have made a bit of progress and I think it's looking better than when I started, hell even the base is warped. I managed to lose the plastic sheen overall and have added some stick by stick lumber loads in the racks. It will also be lighted when I'm done.*

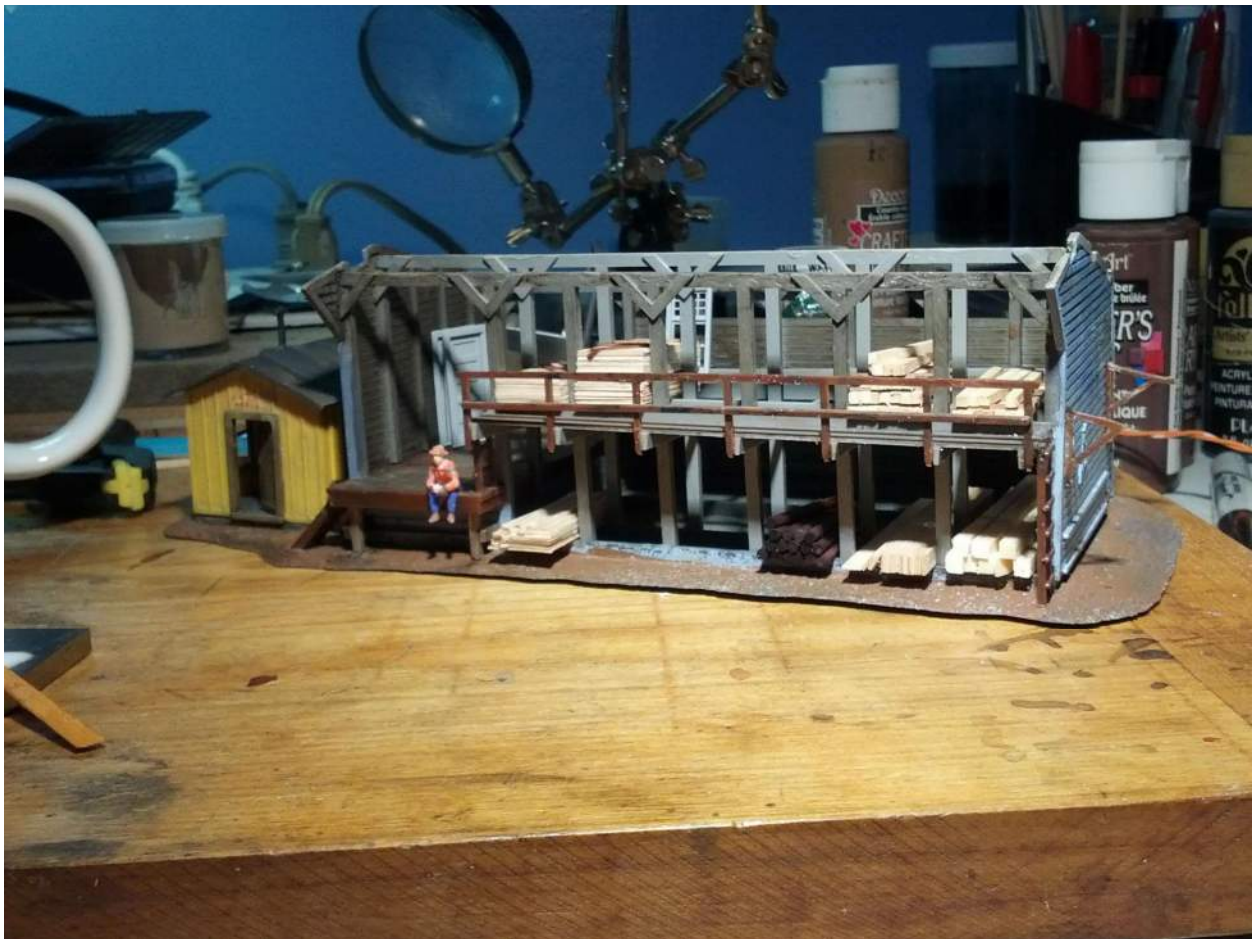


Photo credit: Harold Kemp

Then Harold sent us this update with photos:

*Well for the last little while I have been working on an old assembled Atlas Lumber yard kit that was in rough shape. First I gave it a thorough cleaning, filled in some of the missing pieces and added interior lights. Then it was weathered and loads were made in scale lumber plus a bunch of other miscellaneous items that might be found in such a structure.*



Photo credit: Harold Kemp





Photo credit: Harold Kemp

20. **Bernard Hellen – 3D Printed Model Railroad Animals:** Bernard has been working from home like many of us and has been using his new found “extra” time to keep on designing in 3D. He has even managed to start a new business from home with this and explains how below:

*I model the G&W Quebec Gatineau shortline railroad from Montreal to Quebec City. I've always loved the scenery aspect of the model railroading hobby. Super-detailing a scene to create a prototypically accurate miniworld is a wonderfully obsessive trip down the rabbit hole that occupies many enjoyable hours. Recently due to COVID-19, like many of us, I've discovered extra time to model and have started a mini business selling my 3d printed animals. Thus far I have listed 21 animals including beavers, wolves, eagles, foxes, moose, mountain goats, rabbits and caribou. You can see them all at <https://miniprints.ca/>*

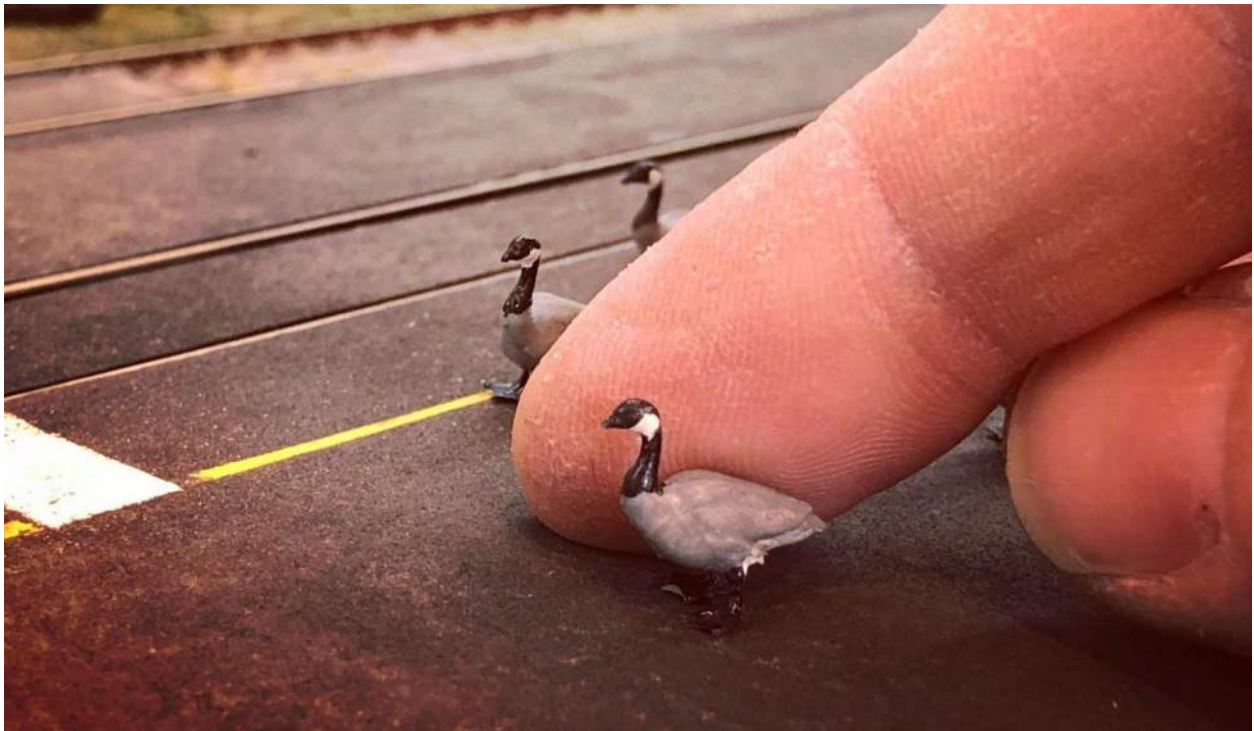


Photo credits: Bernard Hellen



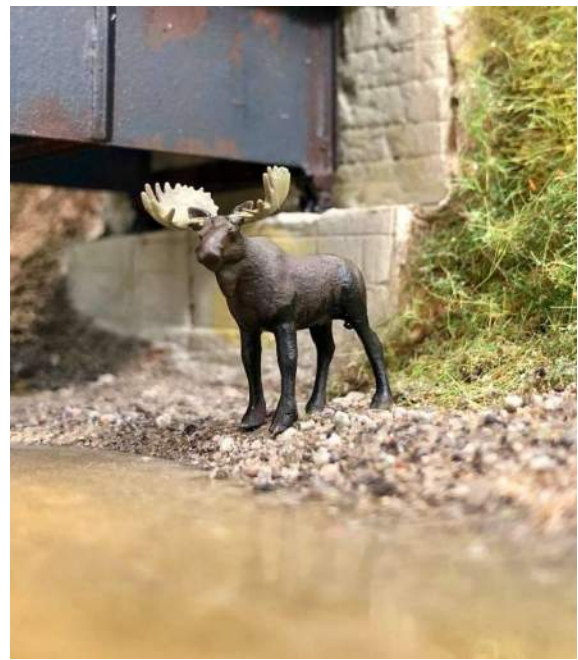


Photo credits: Bernard Hellen



21. **David Hicks – A Window Solution And Double Lift Track Bridge:** In our last letter David wrote to us about renovating his layout to extend his city and in this issue he shows us how he solved a layout verse window issue (and I get to write: “Double Lift Track Bridge” ...Okay, David, please send us a few pictures of the *Double Lift Track Bridge* for the next issue! Does it work as well?) David describes his solution below:

*The bridge across the window is four relatively cheap lifelike bridges painted & weathered. It enables full light & opening in the summer.*



Photo credits: David Hicks

*The lift bridge is a Walters single track widened to a double track. This was actually a very easy conversion. No adjustments were required for the sides, the cross beams were spliced, the remaining 50% were made from readily available styrene parts. The cement weight was expanded from flat styrene.*



Photo credits: David Hicks



22. **Richard Morrison – Clarifying The Turnouts:** Like Willie and many others, Richard is also making changes to his layout during this time to improve operations and he is also making sure everyone on the layout has a job to do:

*James: I have more than a dozen turnouts in my yard controlled by toggles on a panel. It's tricky to figure out which toggle throws which turnout. I wanted to put numbers beside each switch, but that looked silly. Instead, my brakemen get full hourly pay to stand next to a turnout and hold up a big sign.*



Photo credit: Richard Morrison





Photo credit: Richard Morrison

23. **Ed Freeman – Detailing Boston & Maine Engines:** Ed shared this photo of the recent work he has been doing on some new engines:

*Have been painting and decaling a couple of Boston & Maine engines. Here is my switcher minus the front decal due to the fact I have applied all the handgrabs in the past and to place that decal over them now is beyond my capabilities. Working on converting an RS3 into an RS2 with many mistakes and hoping the situation improves so that I can order parts to replace those I have ruined. Cheers*



Photo credit: Ed Freeman



24. **Willie Waithe – The Road Into MacMillan Yard:** After Willie finished his turnout repairs in the MacMillan Yard he wrote: “*I now notice that there are vehicles in the yard but there is no through road for them to get there!*” (Hmmm, what did Hal say about “layout completion”?). Willie updates us with:

*Hello fellow modellers: I hope you are all well and surviving the isolation. To keep you informed, here is a photograph of the road into the yard. I used the video of the real yard as a guide to the structures and colours of the yard and I am now doing further work on the yard (landscaping, buildings etc). Here is a link to the section of the video showing the road: <https://www.youtube.com/watch?v=LdynjfM-7FI> I am looking forward to welcoming you back eventually.*



Photo credits: Willie Waithe



25. **Gerald Harper – Portable Layout “Parr - St Blazey”**: In our last letter, Willie told us about some changes he wanted to do for some turnouts in MacMillan Yard after observing the locomotives passing over them in the last operating sessions. Willie give us an update on the progress here and writes:

*I have just finished tweaking the painting of the backdrops on my super portable layout called "Parr - St Blazey" which refers to a branch line in Cornwall serving a clay mine to the nearby dock. It combines O scale and On30 scale. I had it booked for display at 3 shows this spring but all have been cancelled so now it may be displayed at 2 in the fall, if they aren't cancelled.*



Photo credit: Gerald Harper



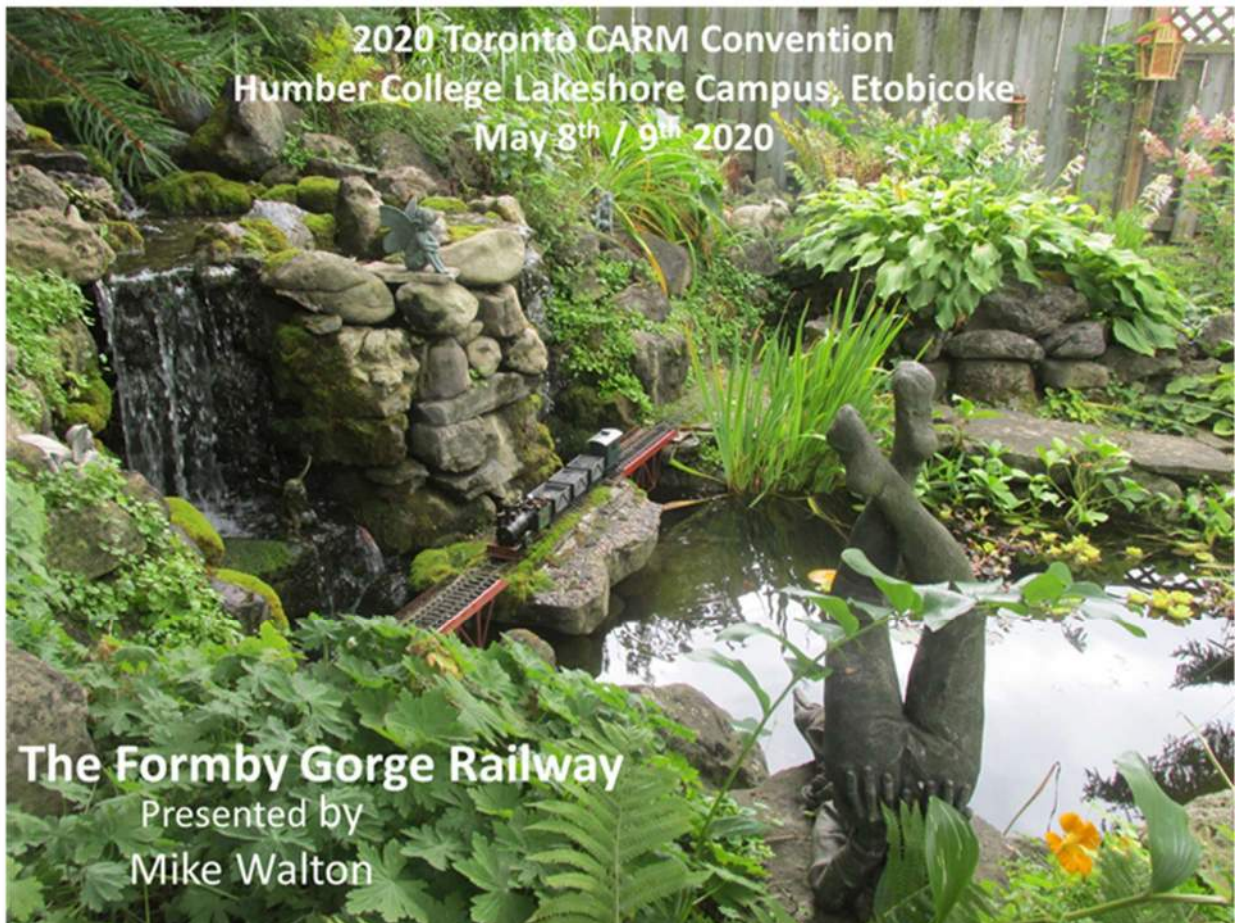
Photo credit: Gerald Harper



**26. Mike Walton – Formby Gorge Spring Survey:** (Note: this is our first Garden Railway submission – thank you Mike!)

Mike is taking full advantage of the downtime to do some changes to his garden layout and he sends this report:

*With the fine weather I took the opportunity to check the levels of the tracks on the Formby Gorge railway in order to take out some of the humps which have grown into the trackbed over the past 20 years. These humps are mainly result from topsoil being added to the flowerbeds every year ☺.*



*Mike writes:* This was to be the starting slide for my presentation at the CARM convention.

Photo credit: Mike Walton



Using a laser level, 29 elevations were measured at about 5 foot intervals around the circular trackwork shown in the diagram below.

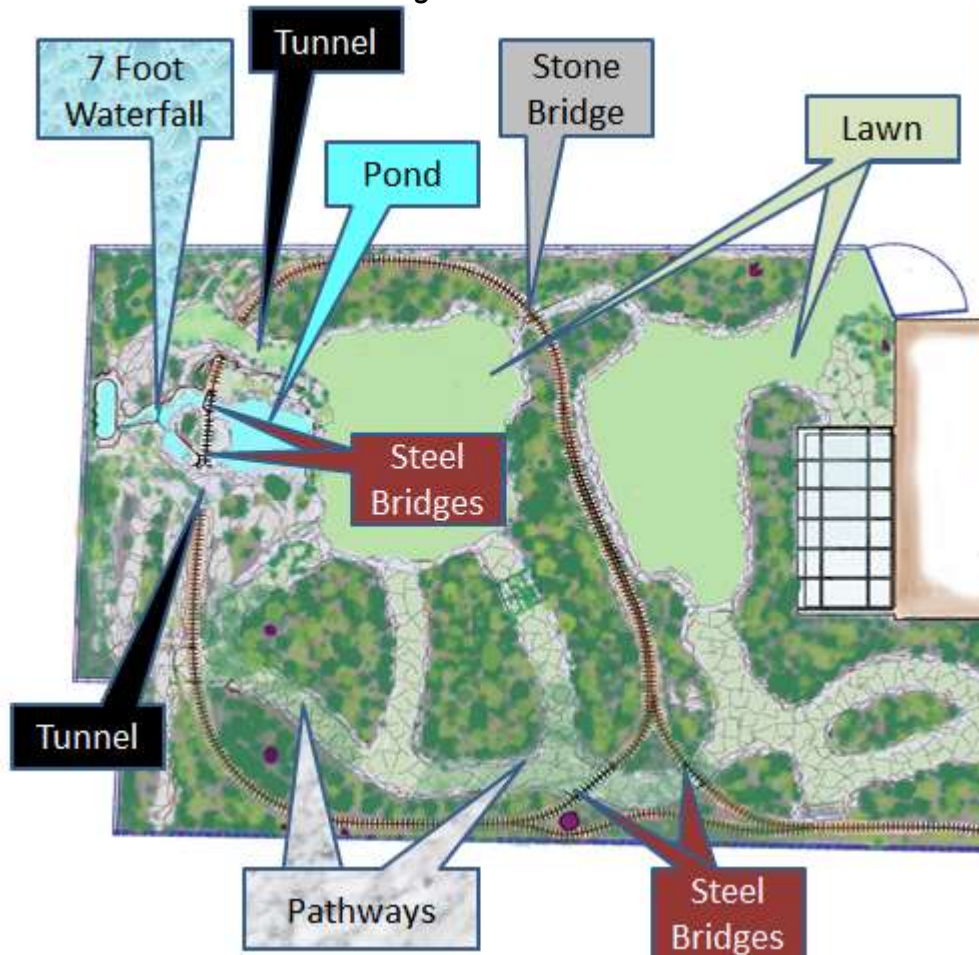


Image credit: Mike Walton

The results were plotted and the corrections planned as shown below

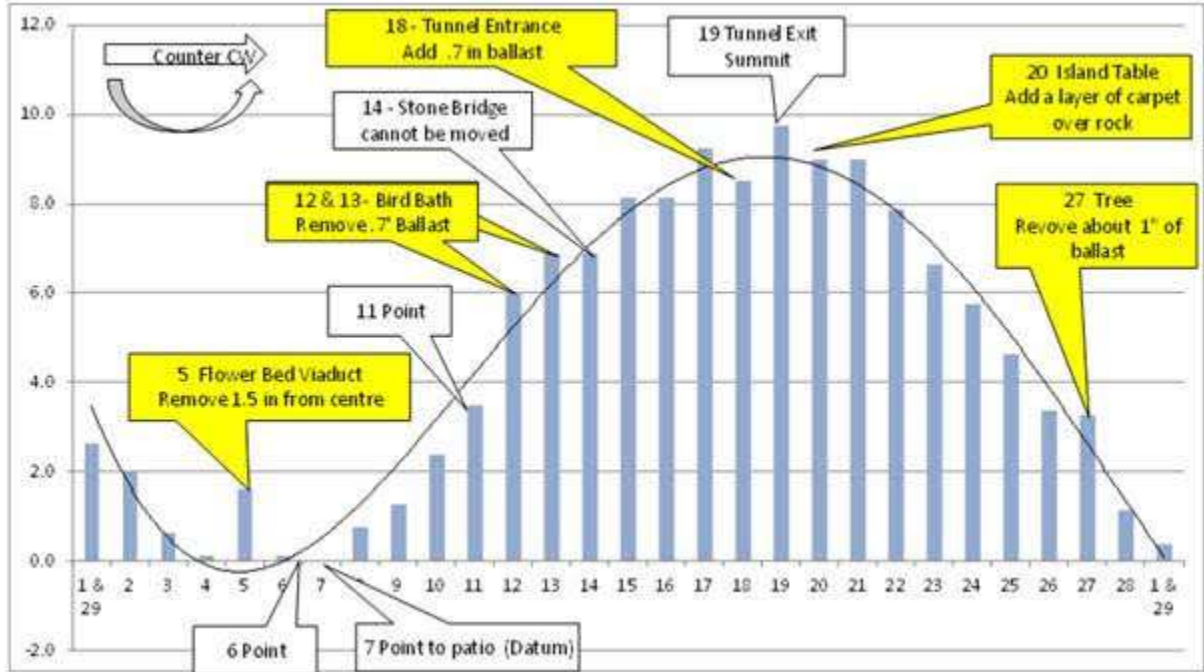


Image credit: Mike Walton

*It's vital that these measurements are taken before the triffids move in J.*

Cheers

Mike

27. **Richard Morrison – Freelance Railway:** Richard is changing his railway to a freelance railway and he writes about that decision below:

*I used to model the Union Pacific, but geographical and historical (and architectural) accuracy is a headache. You can freelance a fictional railroad any way you want. Since we live in the northeast corner of the Guildwood Village area of Scarborough, I thought it would be fun to name the railroad after our neighbourhood. Bill Brillinger in Manitoba made me up a sheet of custom decals, enough to decorate 50 cars of locos for just \$75.*



Photo credit: Richard Morrison



**28. David Woodhead – Industrial Scene and Website:** David has been finishing an industrial scene on his layout that he indicates he has not had time to do yet. He also sent us a link to follow to his layout's webpage to see more pictures of his layout. He writes:

*I tried to get all my projects in one picture! Here's an industrial scene on my On3 layout that had languished unfinished – I used some kitbashed Korber and Chooch buildings with scratchbuilt shallow relief fronts.*



Photo credit: David Woodhead

*In front is a Bachmann Heisler converted to On3 with a new brass bunker and a Flying Zoo Porter 2-6-0 on which I converted the tender to a six-wheel design as a first step to backdating it to an 1870s look.*



Photo credit: David Woodhead

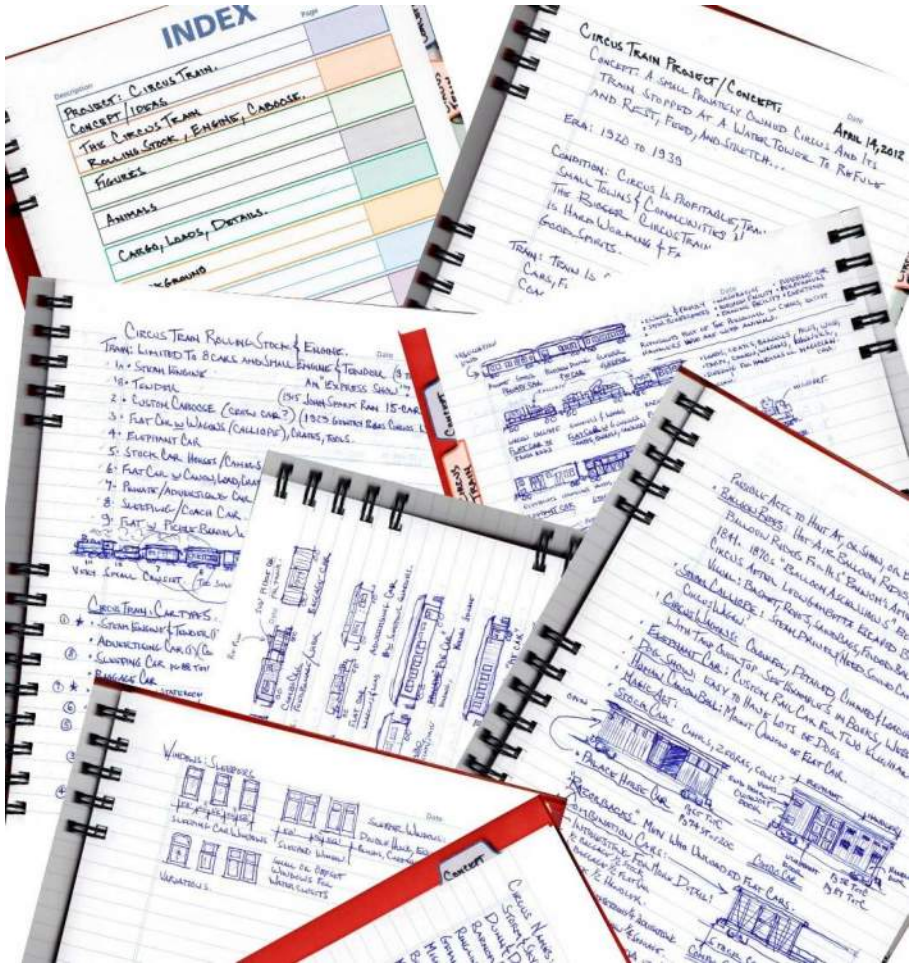
*Hi, James - here's my submission for this time. I'll be updating my railway website, too, at <http://www.davidwoodhead.com/page7.html>*

*Thanks for doing this!*

*David Woodhead*



29. **James Rasor – Circus Train Outline:** I have been doing research for my new “N scale” circus train. This is an “*imagineering*” project for me. It will be a small family owned circus train based around the 1920s just after WWI and before the depression for a 24” T-Trak section. I start all my large projects like this with notes in a sketch book so that I can pick it up at any time and dash some notes in it. I keep notes for everything, including the name of books I look at, the name of websites, the name of manufacturers who have pieces I may be interested in, my ideas and thoughts, and my sketches. Sometimes, things go on hold (sometimes for years!), but when I get back on track, it helps to have all the ideas in one place to re-read and see if I still want to continue with that project. Sometimes the project gets finished ...and sometimes, as Hal said, “*It will never be completely finished*” and the idea stays in the book as just an idea – but the process is part of the fun for me!



Photo, sketches, and notes: James Rasor





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**That's our May 2020 addition** of what some of our CARM Toronto Chapter Members are currently working on.

Now, go look at your stash of projects, or dust off your layout, or grab that railroad book you have been meaning to read from cover to cover, or sort and label your railfanning work, or work on that railroad presentation, or install that first decoder, or write about the first train you ever saw or road on, or teach your family how to be operators, or teach them how to solder or ballast a track, etc., etc., and have fun! It's one of the greatest pastimes that you can enjoy either alone, with friends, or with family ...and it keeps your mind active and healthy!

**And.... don't forget**, we would like to hear from every one of you about what our *CARM Toronto Chapter Members* have been working on so please do send me a photo and a quick 75 word maximum description (*or even just a written description*) about what you are currently working on and we'll get it in the next **CARM Toronto Chapter - What are you working on?** chapter email letter.

**How do you do it:**

**Step 1: Send me a single photo or even just a description of your:**

- present model railroad project,
- or your most recent prized model railroad purchase,
- or your model railroad layout photo or layout drawing,
- or your most recent prized railfan photo,
- or your current model railroad scratch-built project,
- or your favourite model railroad rolling stock,
- or your latest model railroad operating session,
- or your research model railroad sketches,
- or your custom model railroad track build,
- or your first model railroad decoder install,
- or your latest model railroad weathering project,
- or the most recent model railroad disaster that happened to you,
- or your model railroad landscape project,
- or your model railroad lighting project,
- or the model railroad area you just pulled apart to start all over again!
- or any model railroad related item that you have been working on.

- It doesn't have to be perfect, professional, etc., etc., etc., just something you had fun with and want to share with us for this hobby we share together.

**Step 2: Include a brief description**, 75 words maximum; or less if you want, that's okay too!

**Step 3: Don't have a camera? That's okay!** Just send me a 75 word description of what you are working on. You do not always need a picture - the goal is to hear from all of you and we really do want to hear from you!

**Step 4: Updates from you!** Yes, we would love to have updates on items our fellow members have submitted previously for the letter – do let us know your progress since your last submission.

**Step 5: Go ahead, submit something again!** Did you submit last time? Wonderful! Please do so again and again and again! This is about us staying in touch and having fun together, even if it is just through this letter sometimes.

**Step 6: Send it to me at:** [CARMtoronto.jamesrasor@gmail.com](mailto:CARMtoronto.jamesrasor@gmail.com)

I will then take the information, put it together with the other submissions and issue it in the next "***CARM Toronto Chapter - What are you working on?***" email letter along with the other members' news every few months.

Let us know what **you** are working on, have fun, and ***stay in touch!***

Yours truly,

James Rasor, Secretary

**CARM Zone 2 - Toronto Chapter**

For more information on the *Canadian Association of Railway Modellers* go to <http://www.caorm.org/>